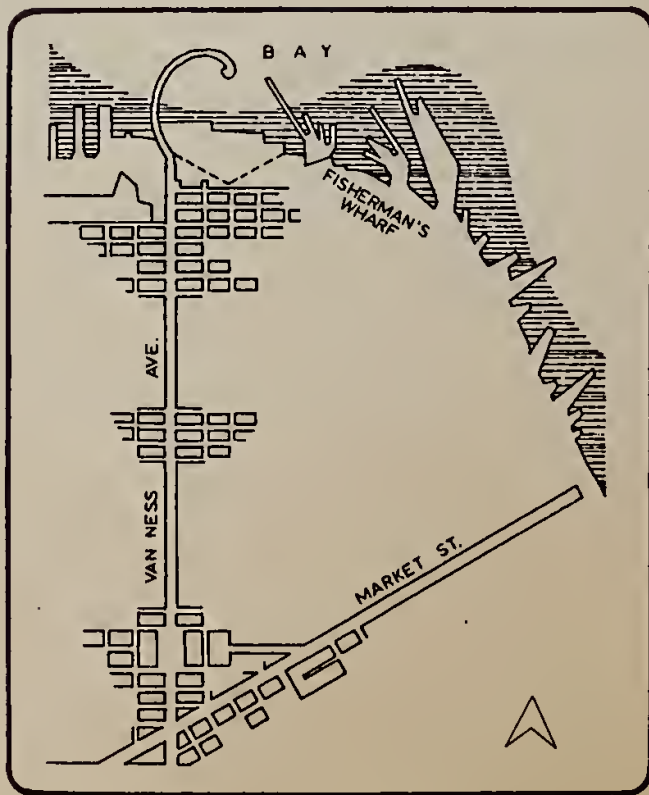


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# VAN NESS AVENUE PLAN



CONSERVATION AND DEVELOPMENT

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Van Ness Avenue plan :  
conservation and  
1983.

## **VAN NESS AVENUE PLAN**

### **Conservation and Development**

#### **I. INTRODUCTION**

In April 1981, the Mayor introduced "A Six-Point Program for Expanding Housing in San Francisco." In her program, the Mayor recommended rezoning certain areas near the downtown to encourage housing development. One of those areas was Van Ness Avenue. In her program, the Mayor envisioned "the future development of the Van Ness/South Van Ness Corridor as a major residential boulevard with mixed-use development stepped back to preserve light and air."

The Van Ness Avenue Plan incorporates a set of land use and urban design guidelines which would facilitate the development of an attractive and livable, mixed use and predominantly residential environment along Van Ness Avenue.

The Plan for Van Ness Avenue is designed to achieve four goals.

#### **MORE HOUSING**

- o To encourage high density residential development within mixed use (residential- commercial) projects along Van Ness Avenue. High density mixed use development can maximize the number of living units to be accommodated on each site while providing a building podium of commercial activity which would serve as a buffer between upper level residential spaces and the more active pedestrian and auto movements at street level.

If, from McAllister to Bay Street, the sites which are not already substantially developed were to be developed under this Plan within the next 10 to 15 years and the average unit size was similar to that of Opera Plaza, about 5,800 new housing units could be provided, along with about 1.8 million square feet of new commercial space.

#### **INTRODUCE A SPECIAL VISUAL CHARACTER**

- o To encourage development which contributes positively to the visual and urban design quality of the avenue. Strong articulation of building bases with setbacks along Van Ness Avenue at the four to six story level and building heights which follow the natural contours of the avenue would improve the visual quality and identity of the avenue.

#### **BETTER PEDESTRIAN ENVIRONMENT**

- o To preserve and enhance the pedestrian environment. Uniform tree plantings along the length of the avenue, along with attractive street furniture and small scale pedestrian-oriented retail activity at ground floor levels would enrich the pedestrian environment while providing important green spaces and neighborhood-serving goods and services.

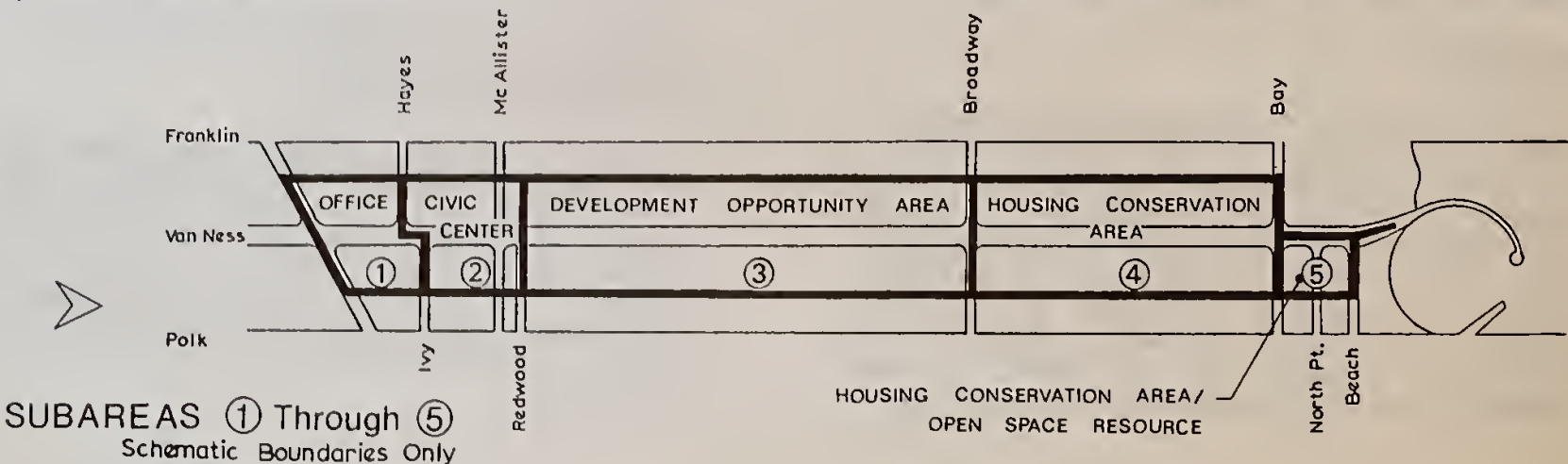


## SAVE SIGNIFICANT BUILDINGS

- o To preserve architecturally significant buildings. Preservation of architecturally outstanding buildings and the design of newer buildings to complement these older buildings of merit would enhance the unique architectural character and identity of the avenue.

The Plan for Van Ness Avenue subdivides the avenue into five discrete subareas which share common resources and characteristics. Some subareas are considered appropriate for major new development while others are more appropriate for conservation with carefully designed infill development.

The Plan describes general "areawide" policies which are designed to create a common character and identity for the avenue by applying urban design and residential livability standards. The Plan also describes specific development policies for each of the five "subareas".



## II. SUMMARY OF RECOMMENDATIONS

With proper land use and urban design guidelines and controls, as well as some public investments in landscaping and related capital improvements, the Department envisions an exciting renaissance for the Van Ness corridor and the transformation of Van Ness into an attractive residential boulevard. The Department believes that specially designed area controls could both stimulate new investment over a five to ten year period and would insure the continuity and quality of compatible uses and urban design in new and older buildings.

The Van Ness Avenue Plan proposes the establishment of a Van Ness Avenue Mixed Use District with specific controls for each subarea. The Plan's proposed regulations are summarized below:

1. A Van Ness Avenue Mixed Use District would be established which incorporates variable land use and density controls for different subareas within the Van Ness corridor.

2. Housing would be encouraged from Redwood Street (the street between McAllister and Golden Gate) to Bay Street by limiting the amount of commercial space that could be built to one third of the amount of residential space provided. This is a change from the present commercial (C-2) district controls which allow housing but also allow only commercial uses to be built. The transformation of Van Ness into a predominantly residential boulevard necessarily requires limitation of the amount of new commercial space to be developed along the avenue, particularly non-local serving commercial activity. A substantial increase in commercial space, particularly office space, could increase traffic and noise volumes during peak periods beyond an acceptable level for the residential environment.

From Redwood Street to Broadway, the amount of commercial space allowed would be related to the amount of residential space provided. Under the regulations proposed, one square foot of commercial space would be allowed for every three square feet of residential space built. Residential space could exceed the 3:1 ratio as long as ground floor retail activity were provided along the Van Ness frontage. Residential use or other, non-retail, commercial use may be provided within the ground floor area so long as pedestrian-oriented retail activity were provided along the Van Ness frontage.

The size of the building would be limited by applicable height and bulk controls. This would replace the existing floor area ratio

(FAR) density control on new commercial development and the unit/increment of lot area control on residential development. Eliminating the FAR and unit density controls, leaving height and bulk limits as the density control, would produce a more attractive urban form for the avenue and a more livable residential environment, while maximizing the amount of housing provided and allowing greater flexibility in the design of individual buildings. (Under the proposed controls, new development within the Redwood to Broadway subarea would be able to achieve an average building area equivalent to a 7:1 FAR.)

3. From Redwood Street to Broadway, the residential unit densities of each parcel would be based on building volume established by height and bulk controls rather than a specification of the number of dwelling units allowed per increment of lot area. Minimum unit size would be established by the building code. A design review procedure would be established to determine size and mix of housing units within individual projects.
4. Existing height limits north of Redwood, which follow the natural contour of the land, would be maintained. Bulk controls for the area between Redwood and a point approximately midway between Lombard and Chestnut Streets would be revised to prevent building forms and massing which obstruct significant view corridors, block sunlight to public open spaces, or create an unattractive skyline or a "canyon effect" at street level which would overwhelm the pedestrian.

5. Height limits from Market to Redwood Streets would be lowered to maintain the visibility and prominence of the City Hall dome. Tall buildings at the Market Street/Van Ness node would serve as a gateway for the avenue and make efficient use of land well served by transit systems.

6. A generally uniform "street wall" or building podium height for Van Ness of 40 to 60 feet would be established. Buildings would be required to be built to the front property line.

Between Redwood and Broadway, additional structure above the Van Ness streetwall podium would be required to be set back an average of thirty feet. Many of the prominent buildings on this portion of Van Ness Avenue are in the 40 to 60 foot height range and the streetwall and setback would enhance this uniform character and prevent the street from being overwhelmed by tall buildings.

7. Conservation of existing housing north of Broadway would be encouraged through a zoning change. The existing commercial (C-2) zoning would be reclassified to a residential - commercial, combined (RC) district which would allow ground floor commercial activity while protecting existing upper level housing from being converted to non-residential use.

8. The provision of affordable housing within individual projects would be encouraged by the proposed variable density controls which facilitate smaller unit sizes. When resident auto ownership is expected to be less than one auto per household, lesser parking requirements for smaller and lower priced living units could be allowed. A design review procedure would be established to determine size and mix of housing units within individual projects.

9. Preservation of architecturally significant buildings, of which there are many, would be carefully monitored. If necessary to make adaptive reuse of these buildings possible, residential use and parking requirements may be modified.

10. To give continuity to the street, a uniform landscape/greenspace plan, incorporating tree plantings, decorative sidewalk treatments and improved street furniture, would be carried out along the entire length of the avenue. Decorative paving and additional landscaping in the median strip would be installed.





VAN NESS AT ELLIS ST. c.1890's

California Historical Society

The location of Van Ness for the mansions was a logical choice. Rincon Hill had become industrialized by John Middleton's Second Street cut, and Nob Hill had only a very few large mansions in the few blocks around California and Mason Streets. Pacific Heights, not yet serviced by the new cable cars, was still considered too distant from the city for concentrated development. Van Ness was close, pleasantly wide, and conveniently underdeveloped. It became popular even for those who could not afford to live there; people would dress up in their finest on Sundays and drive down the shaded avenue in their carriages to "see and be seen" by others doing the same.

### III. VAN NESS AVENUE: PAST AND PRESENT

#### History of Van Ness Avenue

Van Ness Avenue began, like most of San Francisco's streets, as a surveyed boulevard prominent on city maps but undeveloped in the early years. By the early 1870's it was fully graded, and east-west streets that had once terminated at Van Ness from the Bay now extended beyond into the Western Addition. The street became an elegant avenue in the mid-1870's when a row of eucalyptus trees were planted on each side. Attracted by the ambiance provided by the trees and the street's width, the city's wealthier residents began to construct their mansions along Van Ness Avenue.



VAN NESS NORTH FROM HAYES ST. Oct 18 1906

Calif. Hist. Society

Franklin Street, a block to the west, had houses almost as large as those on Van Ness and this area eventually merged into Pacific Heights. Then, as now, Polk Street to the east was the neighborhood's shopping district. It likewise served those who lived on Nob and Russian Hills.

In 1906, soldiers attempting to contain the fire that had ravaged the city after the earthquake attempted a number of fire breaks further east of Van Ness Avenue; all were crossed by the fire. They tried again at Van Ness, dynamiting every building on its east side south of Filbert Street, and were finally successful in containing most of the fire and saving the Western Addition, although the area between Sutter and Washington Streets was not contained until Franklin Street, and the area south of Golden Gate Avenue was not contained until Octavia Street. Afterwards, two business districts formed to replace the burned out downtown -- Fillmore Street and Van Ness



Calif. Hist. Society

VAN NESS, EDDY TO ELLIS ST. 1906

Avenue. Surviving mansions on the west side of Van Ness were converted into stores, and temporary commercial buildings were quickly constructed on the east side of the street. The city's major department stores -- City of Paris, the White House, and the Emporium -- all located here, as did banks such as the Bank of California and the Anglo California Bank.

Within a few years the downtown was rebuilt, and Van Ness was deserted. In The City That Is (1909), Rufus Steel wrote "the rejuvenation of the downtown district left Van Ness Avenue practically forsaken, and it is improbable that that thoroughfare will have any future business importance." As we know now, Mr. Steel was wrong in his prediction. From 1904 to 1908 a number of small auto showrooms and garages had developed along Golden Gate Avenue between Hyde and Van Ness, with others on Van Ness itself. After 1909 the auto showrooms quickly filled the void of departing businesses along Van Ness.



Calif. Hist. Society

VAN NESS, GEARY TO BUSH ST. 1907



Calif. Hist. Society

VAN NESS, BUSH TO SACRAMENTO ST. 1907

Many of the auto sales businesses are proposing to relocate to a new, more concentrated and accessible showroom area within the city. With the relocation of the auto showroom businesses and the ancillary auto service and parts businesses, a number of properties will be available for new development or adaptive reuse.



Because of this use, and much later, the designation of Van Ness as U.S. Highway 101, the avenue became a major vehicular thoroughfare with businesses oriented towards citywide and regional markets rather than those living in surrounding neighborhoods. Presently, only a few blocks in the Civic Center area (much further south than where the mansions stood) and a few auto showrooms, churches, flats and apartment buildings along the avenue give a hint of its earlier elegance.

The automobile showroom activities are declining as a principal use and major economic activity on the Avenue and are beginning to experience strong pressure for conversion to mixed use residential-retail and office activities.





## Van Ness Avenue Today

Van Ness Avenue extends 63 blocks -- some 12,000 feet -- from Market Street north to the Bay. The Plan covers all parcels fronting on the avenue, as well as some abutting properties (See Figures 4 through 8).

Van Ness Avenue has a mixed residential and commercial character. Although residential and commercial uses exist throughout the length of the street, the largest concentration of housing is found in the northern portion of the street and the highest concentration of commercial uses is found in the southern portion.

To better guide new development within the area, five discrete subareas have been identified and are described below.

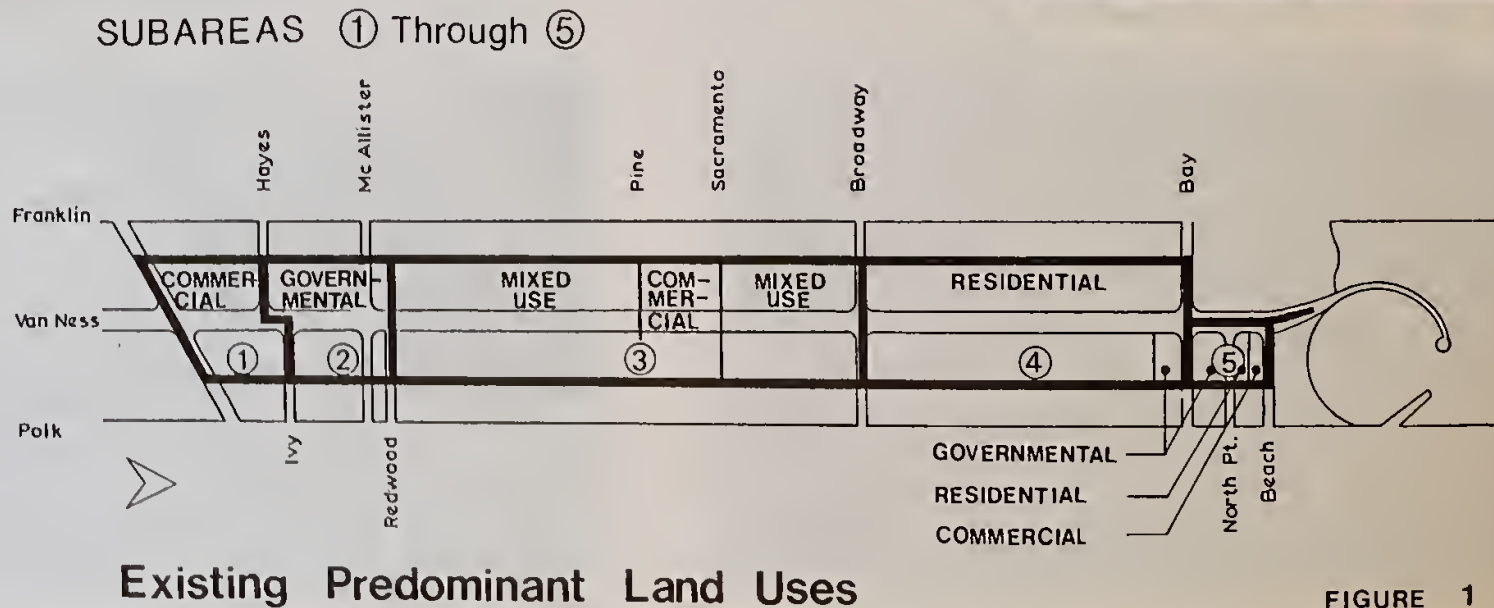
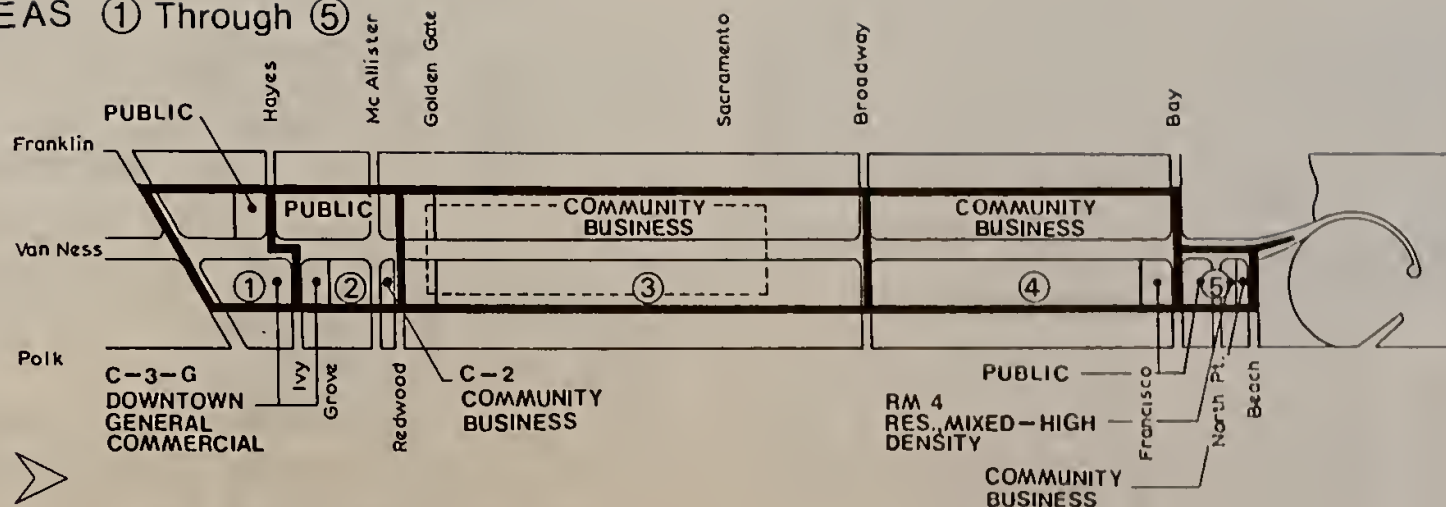


FIGURE 1



## SUBAREAS ① Through ⑤



### Existing Use Districts

□ AUTOMATIVE SPECIAL USE DISTRICT

FIGURE 2

#### Subarea 1: Highrise Office Node (Market to Hayes/Ivy Streets)

Subarea 1 is a rather small area encompassing five and one-half blocks from Market Street north to Hayes and Ivy Streets and from Franklin to Polk Street (See Figure 4). These 5-1/2 blocks serve as an edge separating the high density downtown C-3 district from the low-rise residential/commercial neighborhoods to the south and west and serve as a gateway to Van Ness Avenue.

This subarea is designated a C-3-G (Downtown General Commercial) use district with a 10 to 1 (10:1) Floor Area Ratio (FAR) with the eastern portion designated a 320-I and 130-G Height and Bulk district and the western portion designated a 320-I and 160-H Height and Bulk district.

The subarea includes two highrise and three midrise office buildings, a number of smaller retail and office buildings and a substantial amount of parking. The subarea features two highly rated, architecturally significant buildings, the Masonic Temple at 25 Van Ness and the High School of Commerce at 135 Van Ness, and two apartment buildings containing 64 units. The area is presently underused with respect to the allowable height and intensity of use, with an overall density of 5:1 FAR.

Subarea 1 lies within an area which is being designated a Special Development District in the Department of City Planning's proposed Downtown Plan. This area would be eligible for receipt of development rights transferred from architecturally/significant buildings in the C-3 use districts. The Downtown Plan proposes to

## EXISTING HEIGHT & BULK DISTRICTS

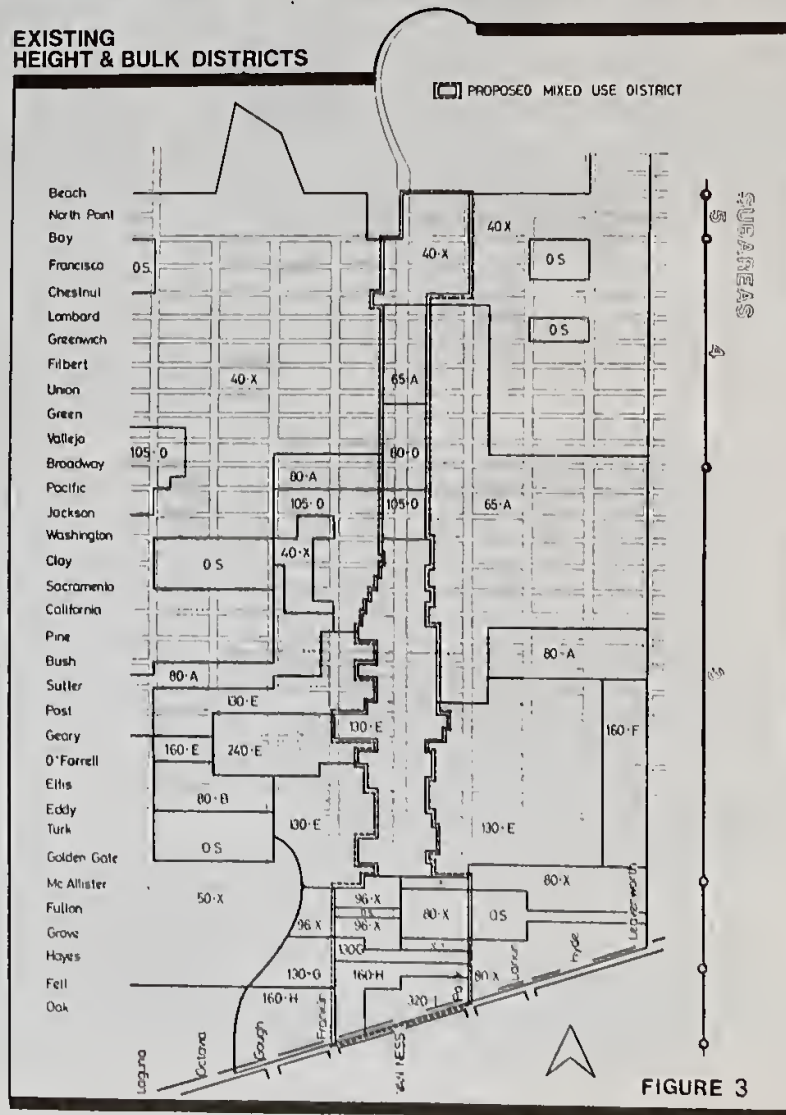


FIGURE 3

Map showing blocks and lots in a neighborhood. Streets include Hayes, Fell, Hickory, Oak, Market, Van Ness, and Ivy. Lots are labeled with 'AB' and numbers. Some lots are shaded with diagonal lines. A north arrow is present in the lower right. A legend at the bottom right explains the 'Assessor's Block/Lot no.' notation.

**Assessor's Block/Lot no.**

- AB 836/all
- 835/all
- 834/1, 3, 4, 18, 19
- 815/1
- 814/all
- 811/2, 3, 22, 10, 12

— SUBAREA BOUNDARY

### \* HEIGHT & BULK DISTRICTS

 SIGNIFICANT BUILDINGS

AB / Lot	Address	Building
834 / 4	25 VAN NESS	MASONIC TEMPLE
815 / 1	135 VAN NESS	HIGH SCHOOL OF COMMERCE BLDG.

Figure 4

## Subarea 2: Civic Center (Hayes to Redwood Streets)

Subarea 2 encompasses portions of six blocks from Hayes to Redwood Streets (the street between McAllister Street and Golden Gate Avenue) and from Franklin to Polk Streets.

Government business and public cultural activities are the predominant land uses within the subarea. The area encompasses City Hall, the Opera House, and the Veterans' Building (which houses the Museum of Modern Art), all of which are official landmarks, and the new Symphony Hall. A new State of California office building is proposed to be constructed on the Van Ness/McAllister/Franklin/Golden Gate block.

Subarea 2 lies within the Civic Center Historic District which is listed in the National Register of Historic Places. The area also includes an architecturally significant apartment building containing 40 units on the north-east corner of Van Ness and McAllister.

A portion of Subarea 2 is included in the Plan for Civic Center which was adopted as an element of the City's Master Plan in 1974. The Plan for Civic Center calls for maintaining and enhancing the Civic Center as a ceremonial focus of community government and culture.

Part of the subarea is zoned C-3-G with a 10:1 FAR and a 130-G and 70-X Height and Bulk designation, part is zoned C-2 (Community Business) with a 4.8:1 FAR, and part is zoned P (Public Use). Allowable height varies from 96 to 80 to 70 feet (see Figure 5). Existing buildings range in height from 10 feet to 80

feet with most buildings in the 4-story category. The Subarea reflects an overall 2.2:1 FAR.

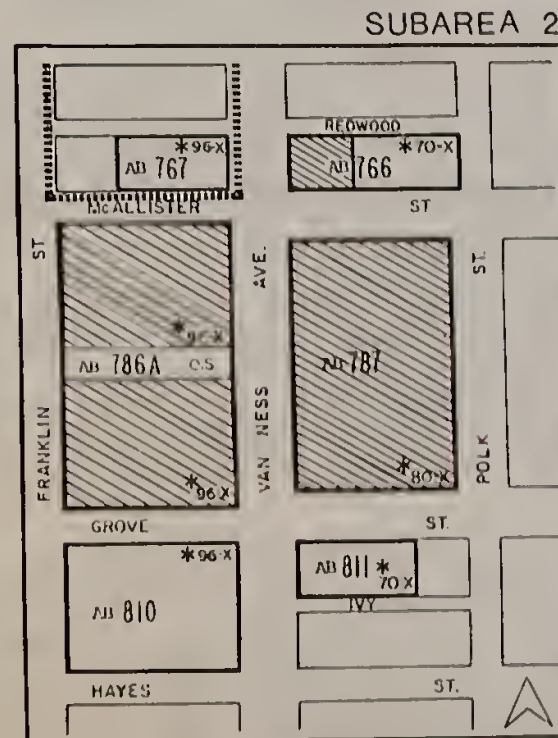


Figure 5



Subarea 3: High Density Mixed-Use Development  
(Redwood Street to Broadway)

Subarea 3 encompasses portions of 33 blocks extending north from Redwood Street to Broadway, and east-west from about the midblock point on both sides of Van Ness towards Franklin and Polk Streets (See Figure 6 for boundary and list of properties). Many of these blocks are bisected by minor streets.

Van Ness Avenue becomes U.S. Highway 101 from Golden Gate Avenue to Lombard Street. As a major thoroughfare, this portion of Van Ness has become a commercial district featuring an auto row, hotels, motels, restaurants, and a variety of other businesses serving city residents and visitors.

This subarea is presently designated a C-2 (Community Business) use district with a 4.8:1 FAR, with much of the area designated an Automobile Special Use District which allows a 10:1 FAR. The Automobile Special Use District ends between Sacramento and Clay Streets and the remaining Van Ness properties are zoned C-2 with a 4.8:1 FAR. Presently, a number of the auto showrooms are proposed to be relocated to another centralized area of the city.

Most of the subarea is designated a 130-E Height and Bulk District. The subarea's designated height limit declines from 130 feet to 80 feet along Van Ness as it approaches the Bay and declines from 130 feet to 65 feet on the eastern side approaching Polk Street.

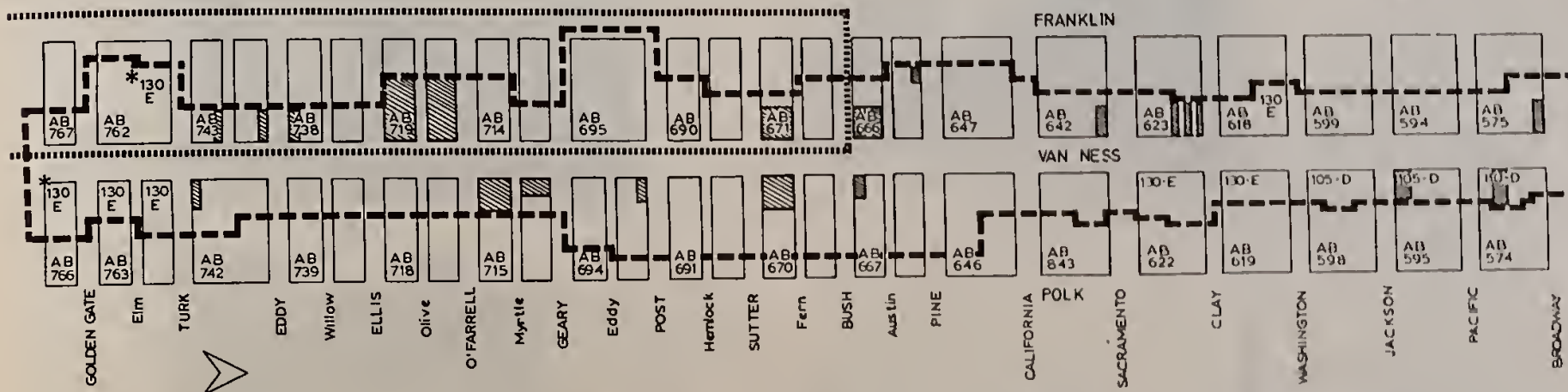
Existing zoning would allow a residential density of 1:200 (one dwelling unit for each 200 gross square feet of lot area) in addition to the 10:1 FAR for properties lying within the Automotive Special Use District. Few parcels have been developed to their maximum limit, most of the buildings being two, three or four stories in height. The west side of Van Ness Avenue from McAllister to Bush Streets lies within the San Francisco Redevelopment Agency's Western Addition A-2 Redevelopment Area. The Redevelopment Plan allows for high density, general commercial activity for this area with high density residential development also a permitted use.

There are 27 highly rated, architecturally significant buildings within the subarea (see Figures 6 and 11 for a listing of these buildings). In addition, there are about 980 living units in 18 buildings (including Opera Plaza) within the subarea.

At California Street, is a four block area occupying the highest elevation of the Van Ness Avenue corridor. This area features two important and unique resources--the cable car terminus and the California Street view corridor which, looking eastward from Gough Street, offers a striking view of the East Bay foot hills and skyline and portions of the Bay Bridge framed by the upper and lower slopes of Nob Hill.



# SUBAREA 3



- SUBAREA BOUNDARY
- ..... REDEVELOPMENT AREA BOUNDARY
- \* HEIGHT & BULK DISTRICTS

## Assessor's Block/Lot No.

AB 767/1,2,3	646/5,8,9,12
766/7,8,9,10	643/13,15,16,17,18
763/8,9,10,11,12,13,14	642/1,3,4,5
762/all	623/1,1A,18,2
743/1,2,3,	622/12,13,18
742/3,4,6,7,8,9,10	619/9,10,11,12
739/5,6,7,8,9,10,11	618/1,1A,18,2
738/3,4,5,17	599/1,8,10,16
719/1,2	598/9,9A,9B,10,10A,10B
718/11,12,13,17,21	595/5,6,8
715/5,9	594/1,2
714/1,2,3,16	575/3,15
695/5,6	574/14/15/16/17/18
694/5,6,7,8,9,9A,10,11,	667/5,7,8,9,10,11
12,13,14,15,16,17	666/1,3,4,5,6,6A,7,9,10,
691/3,5,8	25,26,27,28,29
690/1,1A,13	647/1,2,3,4,7,8,9,10,13,
671/1,2,7	14
670/9,10,11,12,13,14,15	
16,17,18	

## SIGNIFICANT BUILDINGS

AB/Lot	Address	Building
742/6	700 Van Ness	Former Auto Showroom
743/1	799 Van Ness	Former Auto Showroom
743/2	725 Van Ness	Apts.
738/5	801 Van Ness	Apts.
719/2	901 Van Ness	Former Auto Showroom
719/1	999 Van Ness	Cadillac Showroom
715/5	1000 Van Ness	Don Lee Building
715/9	1050 Van Ness	Former Richelle Hotel
694/11	1142 Van Ness	Concordia Club
671/2	1301 Van Ness	Former Goodyear Salesroom
670/13	1300 Van Ness	Regency Theater
667/10	1400 Van Ness	Auto Showroom
666/4	1415 Van Ness	Auto Showroom
666/5	1401 Van Ness	Apts.
666/29	1623/1631 Pine	Salesroom
642/1	1699 Van Ness	Auto Showroom
623/1	1745 Van Ness	Apts.
623/1A	1735 Van Ness	Apts.
623/18	1725 Van Ness	Apts.
595/5	2000 Van Ness	Medical Office Building
575/15	2117 Van Ness	Church
574/16	2134 Van Ness	House

Figure 8

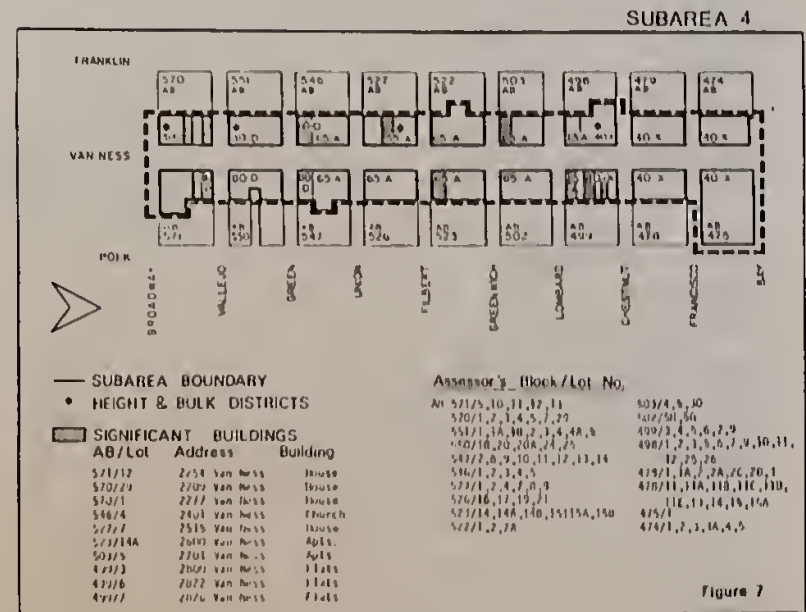


#### Subarea 4: Housing Conservation (Broadway to Bay Street).

Subarea 4 extends nine blocks from Broadway to Bay Street and encompasses portions of 18 city blocks. Zoned C-2 with a 4.8:1 FAR and height limits diminishing from 80 to 40 feet, the primary use is medium-density apartment housing, although a number of older houses have been converted wholly or partially to commercial use. About 970 living units in 68 buildings lie within the subarea (See Figure 7).

Most of the commercially-zoned properties along Van Ness Avenue contain one and two-story buildings where six or eight-story buildings are allowed under existing height limitations. Existing subarea development reflects a 2.7:1 FAR. Residentially-zoned lands are more closely developed to their maximum allowable height and the existing overall subarea residential density reflects a medium density (1:340 or RM-3 equivalent) level of development which is a higher density than would be allowed under the subarea's present zoning designations which range from low (RH-3) to medium (RM-3) density.

There are eight highly rated architecturally significant buildings within the subarea (See Figures 7 and 11).



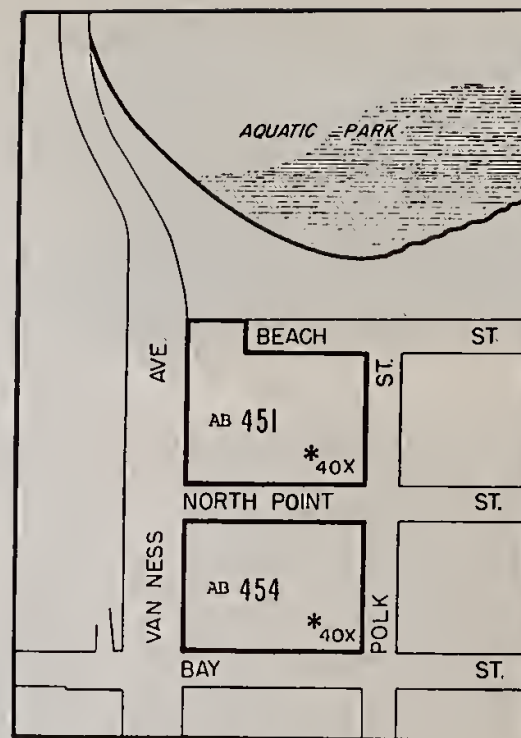


Subarea 5: Visual Node and Open Space (Bay Street to the Bay Shoreline)

Subarea 5 extends from Bay Street to the San Francisco Bay shoreline (See Figure 8). One block of the subarea is devoted entirely to public use (Galileo High School gymnasium and play field); the other block includes two 18-story residential apartment towers (the Fontana) zoned RM-4 (residential mixed, high density) and an office building (the Eastman Kodak Building), zoned C-2; on the west is Fort Mason. Portions of the area are under the jurisdiction of the Golden Gate National Recreation Area.

The most striking resource is the public right-of-way which extends from the Van Ness Avenue/Bay Street intersection north to the Bay shoreline and the Municipal Pier. It offers a spectacular view of the Bay and its islands and the hills beyond. The visitor to this area is afforded a panoramic view, moving from an exciting cityscape to the more soft and majestic forms of the Bay waters and the lush Marin headlands. However, much of the wide concrete and asphalt right-of-way is devoted to parking (200 + spaces) which depreciates the area's open spaces and scenic vistas.

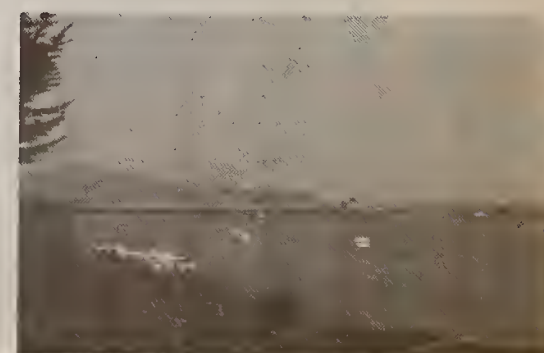
SUBAREA 5



— SUBAREA BOUNDARY  
 \* HEIGHT & BULK DISTRICTS  
 Assessor's Block/Lot No.  
 AB 454/1  
 451/4,5,6



Figure 8





#### IV. OBJECTIVES AND POLICIES FOR NEW DEVELOPMENT

In order to provide continuity of development and create a special identity for the avenue, the following objectives and policies would apply to all developments within the Van Ness corridor. Specific subarea objectives and policies are presented after the areawide policies.

New developments and conversions of existing uses within the Van Ness Avenue area would be subject to the general areawide guidelines as well as to any policies and controls developed for the specific subarea within which the project lies. Where controls vary or conflict, the controls for the specific subarea would govern.

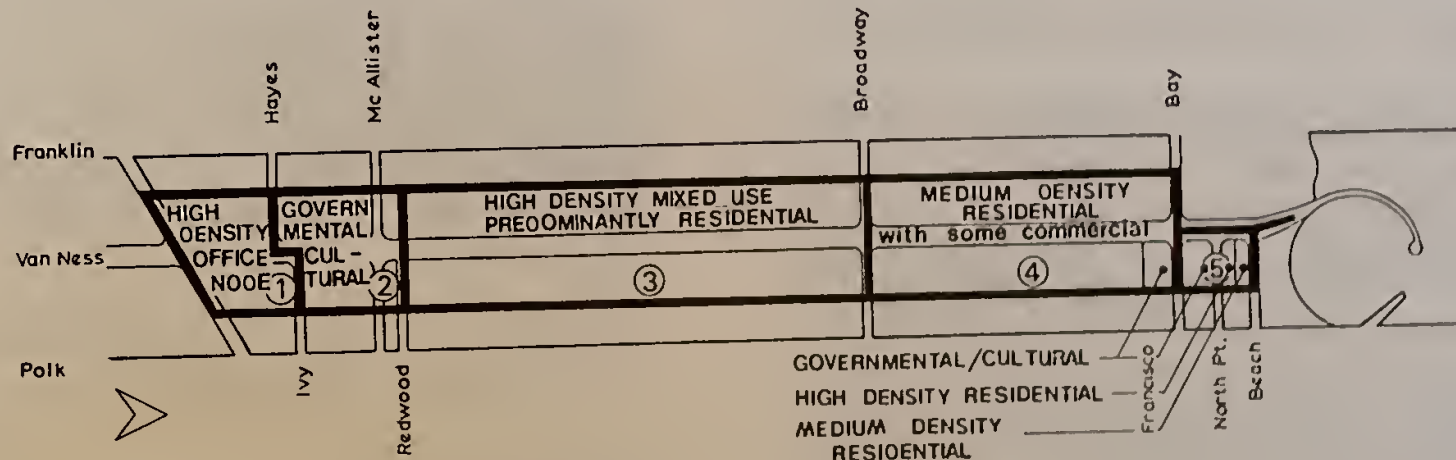
#### LAND USE

##### Areawide Objectives and Policies

**OBJECTIVE 1:** To Create a High Density Mixed-Use, Heavily Residential Boulevard.

**Policy 1:** Provide a mix of retail, service and office commercial uses at ground floor and lower levels with residential space dominating the upper levels.

Single use buildings (all commercial or all residential) are appropriate at the northern and southern ends of the avenue (Subareas 4 and 5 - Broadway to North Point - and Subareas 1 and 2 - Market to Redwood), and in other areas only where such use would make feasible the preservation of architecturally significant buildings (See Figure 11).



**Proposed Land Uses  
BY SUBAREA**

**FIGURE 9**

Subarea 3 (Redwood to Broadway) should feature high density residential development with some commercial space allowed within the podium level and with retail or personal service activities required along the Van Ness Avenue frontage at the ground floor. Within this subarea, a minimum ratio of three square feet of residential space should be developed for every square foot of commercial space provided, including the required ground floor retail space.

Policy 2: Maximize the Number of Housing Units.

An overall mix of unit sizes on Van Ness Avenue is desirable. However, because of the location the emphasis should be on a larger number of small and medium sized units (studio, 1 and 2 bedroom) rather than a small number of large size units. Construction of rental housing is encouraged.

Policy 3: Limit the Provision of On-site Parking.

Minimizing on-site parking required for residential developments could help reduce unit costs, maximize the number of residential units, as well as encourage transit use. If it can be established that auto ownership among the existing residents of Van Ness Avenue is less than one vehicle per household, or the ownership of prospective residents is likely to be less, then the one-for-one parking ratio should be lowered. Lesser parking requirements would be particularly appropriate for smaller sized units (studios and 1 bedroom). Any reduction in parking would be expected to be translated into lower priced housing units because of the lower land and construction cost allowable to such units. A cooperative or congregate off-site

parking facility shared with residents of other nearby projects and located within a reasonable walking distance would be desirable.

Policy 4: Encourage large lot development.

There are over 230 parcels along the 63 block faces fronting on Van Ness Avenue. Most of these parcels are small and are in separate ownerships. Assembly of small parcels to facilitate large lot development, particularly in Subarea 3, can better incorporate circulation and urban design features which would contribute to the transformation of the street into a residential boulevard.

Policy 5: Employ various techniques to provide more affordable housing.

The Plan allows broad design flexibility as to unit size, allowing the creation of small, compact units as a means of lowering unit cost. A number of design measures such as smaller units, "street-facing" or "no view" units and units with quality building materials yet lower cost appliances and carpeting can reduce unit costs and prices and can be combined with available housing assistance programs to lower housing costs.

The cost savings associated with the relaxation of parking requirements should be translated into more affordable residential units.

## Subarea Land Use Objectives and Policies

### Subarea 1: High-Rise Office Node (Market to Hayes/Ivy Streets)

OBJECTIVE: To Create a Node of Intensive, Predominantly Office Activity.

#### Policies:

Policy 1: Encourage intensive office use above ground floor retail activity or mixed retail/office/residential activity in new development and major renovation of existing buildings. Provide retail and personal service activities along the Van Ness frontage with other, less intensive uses provided in the interior space.

Policy 2: Create in a portion of the subarea a Special Development District to accommodate high density development.

The purpose of the Special Development District would be to enable the proposed C-3-G base 8:1 FAR to be increased to a C-3-O base FAR and beyond (to the limits set by height and bulk rules) through the transfer of development rights (TDR) from elsewhere in the city's C-3 districts. This will provide the means to take advantage of the allowable height at the Market - Van Ness intersection and create a formal gateway to the Avenue.

### Subarea 2: Civic Center (Hayes to Redwood Streets)

OBJECTIVE: To Maintain and Reinforce the Civic Center as a Ceremonial Focus of Community Government and Culture.

#### Policies:

Policy 1: Maintain existing use controls.

Policy 2: Preserve existing significant buildings and infill with new development which complements and enhances the various business, cultural and social functions of the Civic Center area.

The construction of the proposed new state office building on the northwest corner of McAllister and Van Ness will complete an important corner of the Civic Center. Within the northern and southern portions of the subarea lie several parcels which can accommodate new development. These sites are designated for administrative, entertainment or parking-oriented governmental functions in the Civic Center Plan portion of the City's Master Plan. However, the apartment building at the northeast corner of McAllister and Van Ness should remain in residential use.

### Subarea 3: High Density Mixed Use Development (Redwood Street to Broadway)

Subarea 3 is one of the few areas in the city where new housing can be accommodated with minimal impacts on existing residential neighborhoods and public services.

Because of this subarea's tremendous potential and its present underuse, a set of guidelines and controls are proposed which are intended to maximize the area's development potential while preserving and enhancing the city's unique and special visual, cultural and recreational resources.



Some of the features that make Subarea 3 attractive for high density mixed use development include:

- o This 16 block strip along Van Ness Avenue maintains a "central place" location and identity. The area is close to the city's major employment center, is well-served by transit, has well developed infrastructure (roadway, water, sewer and other public services), wide roadway (93+ feet) and sidewalks (16+ feet), has continuous commercial frontage and numerous attractive, architecturally outstanding buildings.
- o There are a number of large parcels which are substantially underdeveloped with respect to current height and floor area controls.
- o The present 130-foot height limitation would allow sufficient floor area on specific parcels, particularly large parcels, to make development of underused parcels economically feasible.
- o The minor streets which bisect most of the blocks within this subarea facilitate access to and from new developments with minimal affects on levels of service of major east-west thoroughfares or on Van Ness Avenue.

Development of high density, mixed-use projects within Subarea 3 would provide the key "anchor"-- the critical mass, scale, and urban design treatment which would facilitate the transformation of Van Ness Avenue into a residential boulevard.

OBJECTIVE 1: To Create a High Density, Mixed-Use, Predominantly Residential Neighborhood.

Policy 1: Modify existing use controls from Community Business and Automobile Special Use District classifications to a Mixed Use District designation designed to encourage a combination of high density residential development above a podium of commercial uses, and to protect existing dwelling units occupying upper floors.

Policy 2: To induce construction of housing, limit the amount of commercial space allowed to a portion of the amount of residential space provided. Require a minimum of three square feet of residential space to be built for every square foot of commercial space developed.

Consistent with the general requirement that ground floor retail activity be provided along the Van Ness frontage, residential space could exceed the 3:1 ratio. This 3:1 ratio of residential to commercial development, in combination with height and bulk controls, would replace the existing Floor Area Ratio (FAR) density control on new commercial development.

Policy 3: Allow residential densities to be established by building volume rather than the lot size.

The number of units provided within individual projects would depend on the height and bulk of the building, the amount of commercial space provided under the 3:1 minimum housing-to-commercial ratio, the amount of on-site parking provided and the size of the units. Minimum unit size would be established by the building code.



#### Subarea 4: Housing Conservation

OBJECTIVE: To Maintain the Scale and Density of this Residential Neighborhood.

Policy 1: Preserve existing housing stock and infill with carefully designed, medium density new housing.

New development should be restricted to residential use with neighborhood - serving retail use allowed at the ground floor. This would involve reclassifying properties from community business (C-2) to medium density (1:400) residential with ground floor commercial use permitted (RC-3 equivalent).

Existing small scale commercial uses above the ground floor should be permitted to remain as non-conforming uses for the life of the building. Existing ground floor residential space could be converted to small scale, preferably neighborhood-serving, commercial use provided that upper level residential space and any ground floor or below grade uses which are accessory to and required for the upper level residential uses (loss of parking, open space, rear yard, access ways, other mechanical features) were not adversely affected by such conversion.

#### Subarea 5: Visual Node and Open Space (Bay Street to the Shoreline)

OBJECTIVE: To transform the Area into a Striking and Grand Visual Gateway for the Residential Boulevard.

Policy 1: Retain existing C-2, RM-4 and P land use designations with the additional requirement that new development or major renovation of buildings within the C-2 district provide interesting and attractive, low-intensity, pedestrian-oriented retail activities at the ground floor.

Policy 2: Create a tree-lined and landscaped median strip within the Van Ness street space and plant rows of trees in the sidewalk space.

This greenspace element, which would replace some existing parking spaces, should be designed to "announce" the area's attractive shoreline open space resources and visually direct the visitor to them.

Policy 3: Support National Park Service plans for improvements of the area within the Golden Gate National Recreation Area (GGNRA) boundaries.

The GGNRA General Management Plan calls for the following improvements:

"All of Van Ness Avenue inside the park boundary will be removed and replaced with landscaping. The Sea Scout clubhouse and maintenance docks will also be removed. The Sea Scouts' boats will be moved to the east side of the lagoon, and their programs and meetings will be held in the aquatic center. The food concession at the foot of Van Ness will receive a good sprucing-up. The municipal pier will also get a substantial cleanup and minor improvements such as fish-cleaning stations and restrooms. (It may also require major structural renovation.) Night lighting throughout the area will be upgraded" (Page 38).

## URBAN DESIGN

### Visual Form

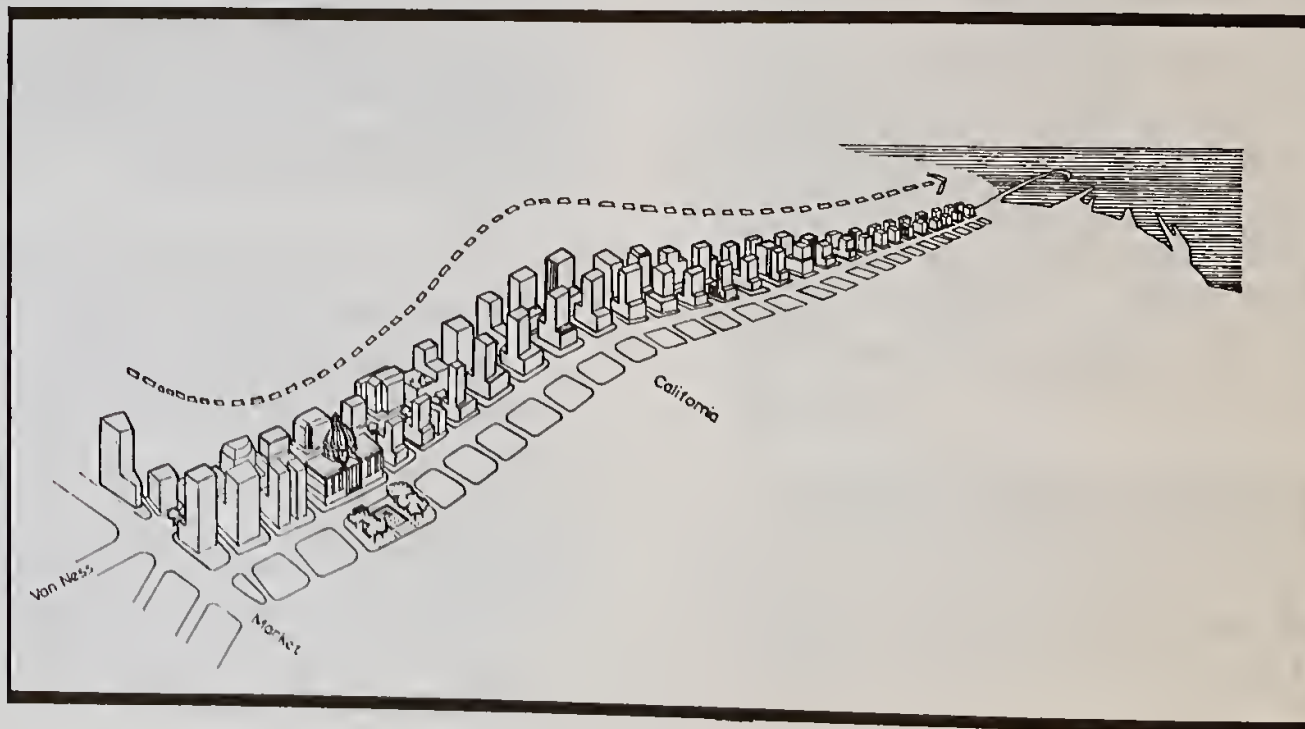
#### Areawide Objectives and Policies

OBJECTIVE 1: To enhance the Natural Land Forms along the Van Ness Corridor with new development.

Policy 1: Maintain height controls which, for the most part, allow sufficient density to encourage and facilitate new development while emphasizing the natural land forms of the area.

A cluster of tall, slender buildings at the Market Street/Van Ness Avenue intersection would provide a focal and entry point for the Avenue while maintaining the continuity of the Market Street commercial activity. This area (Subarea 1) should be limited in building height to 320, 200, 120, 96 or 80 feet (See Figure 10) with the buildings descending in height toward Civic Center.

Height limits should gradually rise from 70 feet in the Civic Center area to 130 feet in the proposed major new development area, Subarea 3 (Redwood to Washington Streets), providing a visual transition which would both maintain the



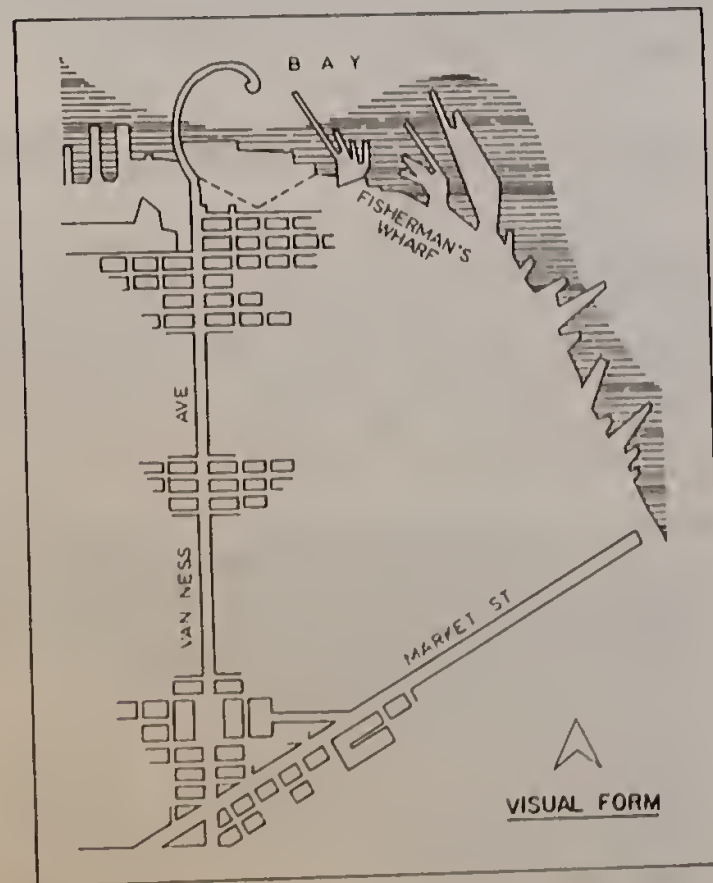
position of Civic Center as a landmark and emphasize the importance of Subarea 3. The gradual tapering of height in steps from 130 feet to 80 feet to 65 feet from the hilltop around California and Pacific Streets to the Bay shoreline would visually express the natural landform while preserving views of the Bay, and maintaining the importance of Bay Street as a gateway to Van Ness and the Bay as a key visual and open space resource.



Photo By John Field

OBJECTIVE 2: To Maintain and Enhance the Street's Visual Form and Resources.

Van Ness Avenue contributes to the city's visual form and urban pattern in a number of ways. As a major crosstown thoroughfare, the Avenue serves as a linear edge which both defines and links several distinct neighborhoods and





activity centers. The avenue, linking mid-Market Street to the Bay, defines the western edge of the central core of the city and provides directional orientation within the City's overall urban pattern. The avenue provides access to a number of prominent focal points including landmark buildings, important cultural centers and activities, view corridors and the Bay shoreline. The street features a number of attractive structures which share a similar character and identity and which impart a visual unity that is lacking in other commercial thoroughfares.

Policy 1: Encourage new development closer to the height limit.

New development should be built closer to the maximum height limit in order to contribute to the street's visual prominence within the city's fabric and to enhance the street's own visual form and identity. However, a variety of building heights within the street's maximum height limit is still desirable to avoid a benching effect on the skyline.

Policy 2: Strengthen the area's existing scale as well as emphasize the predominant height of significant buildings by maintaining in the high density mixed use development area (Subarea 3), a generally uniform street wall with a deep setback above this street wall.

For proper framing of the street within the area designated for major new development (Subarea 3: Redwood to Broadway), structures should be built to the street property line along the Van Ness Avenue frontage up to an average podium height

# PROPOSED HEIGHT & BULK DISTRICTS

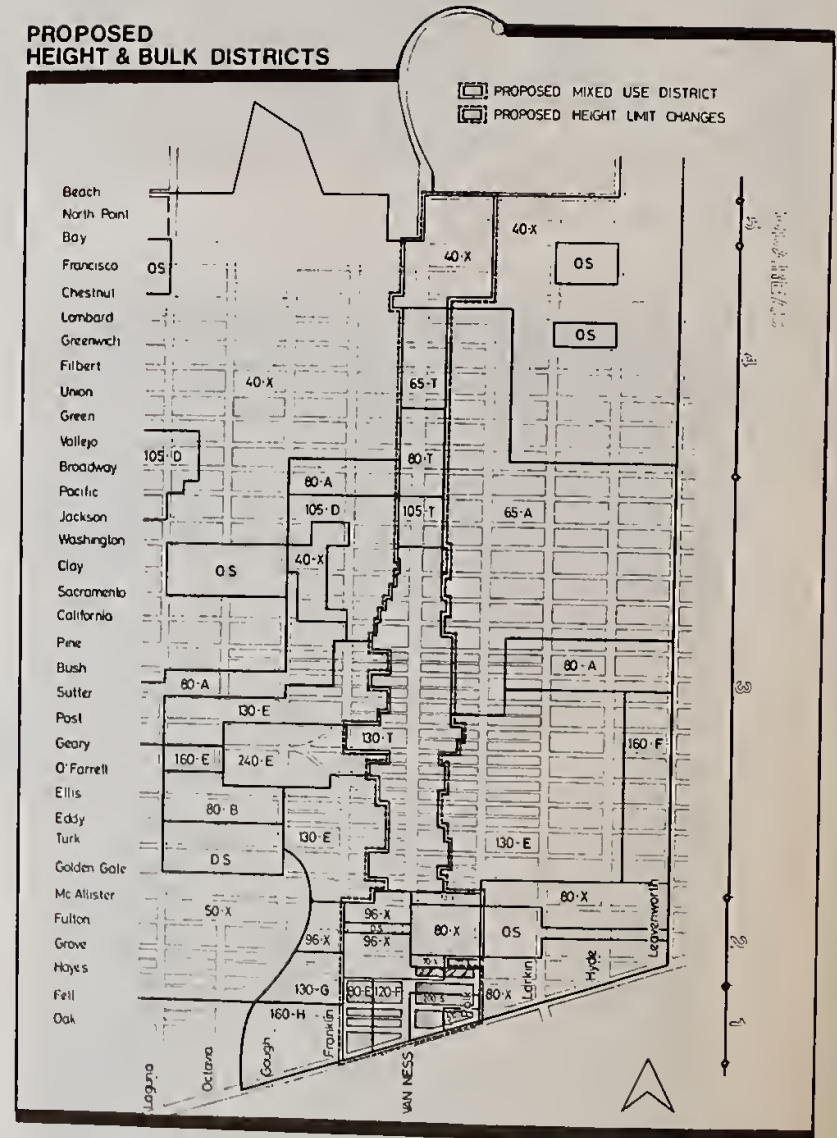


FIGURE 10

of 50 feet at which point a setback should occur. The height of this street wall could be varied between 40 and 60 feet, depending upon the height of adjacent significant buildings. To provide a more graceful transition in the lower scale Subarea 4, a 15 foot setback should be provided at the 40 foot height. Within the street wall, only minor indentations to articulate the building facade should be allowed. An average thirty-foot depth setback above this street wall would serve to create and maintain a consistent and well-proportioned frame for the boulevard. To create some variety, the 15 or 30 foot setback above the podium may be provided in steps which do not rise above the sight line from the opposing sidewalk as long as the steps can and are designed to function as useable open space.

Policy 3: Conform building shapes to bulk controls. In higher height districts require conformity to controls which are designed to encourage sculpturing and articulation of building towers, particularly at the upper levels.

The following controls are proposed for the various bulk districts as shown on the accompanying map (See Figure 10):

TABLE 1  
PROPOSED BULK LIMITS

District Symbol on Bulk Map	Height Above which Maximum Dimensions Apply (in ft.)	Maximum Plan Dimensions (in feet)	
		<u>Length</u>	<u>Diagonal Dimension</u>
O.S. S	See Section 290 of the City Planning Code Controls to be developed as part of the Downtown Plan.		
X	See Section 260(a)3 of the Planning Code		
E	65	110	140
F	80	110	140
T	50	110	140

In bulk district T, the following additional controls should be applied to minimize view blockage while maintaining sun exposure along Van Ness Avenue.

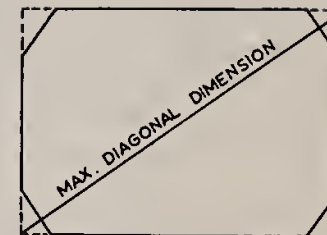
#### BUILDING FORM LIMITS

To encourage articulation of the vertical plane of the building tower and sculpting of the top to produce a more slender skyline form, the maximum allowable square footage of the building should be limited to 80% of what is possible under the bulk limits based on the actual height of the building. Of the 20% reduction required from the maximum building envelope, the predominant amount should occur in the upper portion of the tower. Reductions in bulk from the base would not be eligible towards the bulk reduction requirement.

This requirement that total square footage be less than the maximum permitted under the bulk rules would decrease the incentive to maximize floor area by building a simple rectangular form. A minimum floor size of 50% of the average tower floor size should be required to eliminate the ability to reduce the actual floor area by building a few very small floors and many floors at or near 100% of the bulk limits.

#### MEASUREMENT OF DIAGONAL DIMENSION

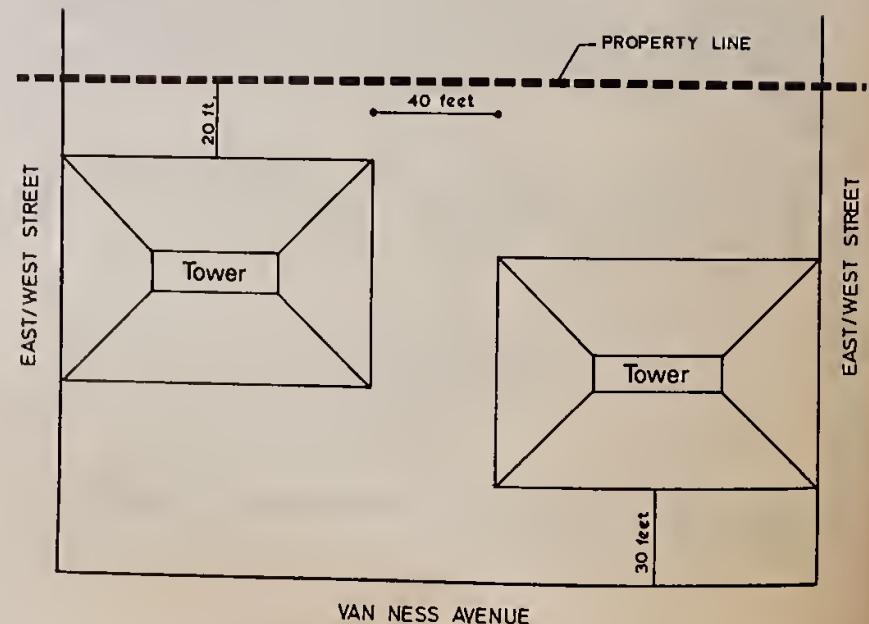
In addition to the provisions of Article 1, Section 102.18 and Article 2.5, Section 271 of the City Planning Code, the diagonal dimension should be measured from a point at which the extensions of two building walls would meet to form a corner if the building walls do not meet in a corner.



#### MINIMUM DISTANCE BETWEEN TOWERS

A minimum distance of 40 feet should be required between towers located within a single project. This distance should be measured on a line perpendicular to the first tower to a line extending from the wall of the second tower.

A minimum setback of 20 feet at the 40 to 60 foot height should be required adjacent to the rear and/or side property line of an interior lot. An exception to this due to practical difficulty or unnecessary hardship may be granted through a variance by the Zoning Administrator.





These requirements are designed to insure that towers are viewed as separate structures thereby reducing their apparent bulk. When towers lack adequate physical separation and differentiation of building materials, texture coloration or massing, they can be perceived as a single structure and the purpose of the bulk requirements is defeated and an excessively massive building results.

#### EXCEPTIONS TO THE BULK LIMITS TO FACILITATE GREATER ARTICULATION OF BUILDING FORM

As a means of encouraging variety in building form and more graceful articulation of building plane and form, an alternate form of building tower conforming to the following bulk limits should be allowed as a Conditional Use, pursuant to Article 2.5, Section 271 and Article 3, Section 303 and all applicable provisions of Section 801.

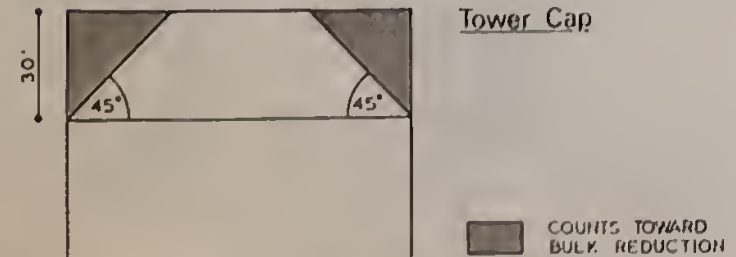
##### Alternate Tower Form

	Height at Which Bulk Controls Become Effective (in feet)	Maximum Length (in feet)	Maximum Diagonal Dimension (in feet)	Maximum permitted Development of Allowable Building Envelope (Percentage of Square Feet of Floor Area)
Base	0- 50	No limit	No limit	N/A
Mid tower	51- 80	125	175	80%; Predominant reduction to occur in the upper portion of the tower.
Upper tower	81-130	90	130	

#### ADDITIONAL HEIGHT ALLOWANCE

Currently the height limit may be exceeded by 16 feet by a penthouse housing mechanical equipment. To encourage more interesting building forms, particularly at the top tower or "tower cap", and to reduce bulk as building height increases, an increase of 30 feet above the "as of right" height should be permitted for mechanical equipment, recreation and open space, and/or residential use, provided that the following requirements are met:

- The mechanical penthouse must be included in this additional space and must be architecturally integrated with the building and screened from view outside the building;
- The additional structure must stay within a plane sloping inward at a 45 degree angle from the outer edge of the building starting at the "as of right" height limit. The area outside the boundaries of the pyramid but within the "as of right" building envelope (the area defined by the bulk and height limits) can be counted towards the 20% building bulk reduction.



## SETBACKS

A thirty foot setback should be required along the Van Ness Avenue property line at the 40 to 60 foot height level. Exceptions to the thirty foot setback requirement along Van Ness Avenue should be granted through a variance by the Zoning Administrator, up to a minimum of 15 feet, in order to permit some stepped forms within a 45 degree plane starting at the 40 to 60 foot height. This would provide for some variation and would improve relationships to existing non-setback buildings. The height of buildings at which setbacks are provided should not be less than 40 feet nor greater than 60 feet.

In addition to the 30 foot setback along the Van Ness Avenue frontage, a thirty foot setback should be required at the 40 to 60 foot height along Pine, Sacramento, Clay and

Washington Streets for properties with frontage along those streets in order to preserve view corridors. Because California is a much wider street, a fifteen foot setback at the 40 to 60 foot height along the California Street frontage would be appropriate. The recommended setbacks on the east-west streets could be varied on a case-by-case basis, through a design review process, as individual buildings undertake massing studies to determine an appropriate building form and setback which would preserve these significant view corridors.

A five foot setback, belt course or cornice, change in materials, or other architectural treatments should be required at the 40 to 60 foot height along Austin, Hemlock and Fern Streets for properties with frontage along those minor streets.





Policy 4: Incorporate exterior building design and treatments in new development which would complement and enhance the street's existing unique Renaissance/Beaux Arts architectural identity.

Policy 5: For large parcel developments with greater than half a block frontages, interrupt facade patterns with a change in architectural treatments, such as changes in fenestration and materials, at least at the half-block interval.

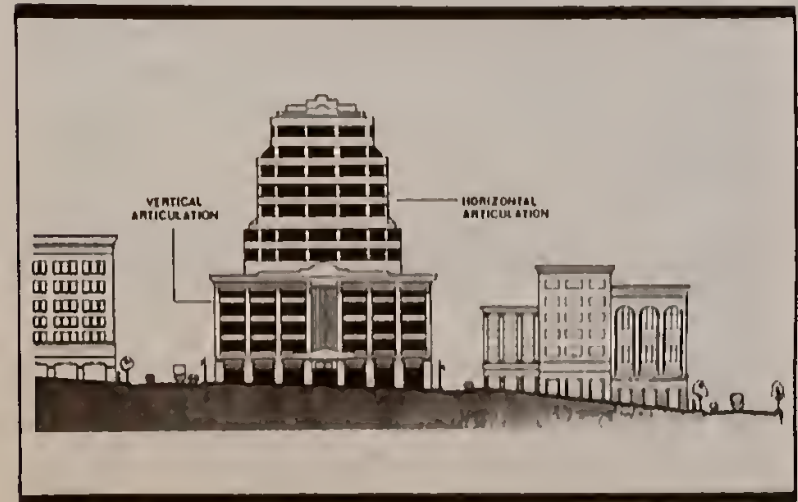
Policy 6: Incorporate design features (such as upper level canopies) on new developments and renovations when necessary to serve as a wind barrier.

#### Streetscape/Building Facade Treatment

**OBJECTIVE:** To Create and Maintain an Attractive, Interesting Streetscape with a Human Scale.

Although Van Ness Avenue is a wide street, and can accommodate large structures, undifferentiated buildings developed to their maximum height could overwhelm the viewer and create an inhospitable pedestrian environment. Therefore, the massing and articulation of buildings should create a human scale and sense of place at street level. Streets should be interesting and pleasant places to walk. The elements of the streetscape which create a positive human scale include active ground floor uses and interesting architectural features within the limits of peripheral vision. Architecturally defined bases on tall buildings also help to create a comfortable human scale.

Policy 1: Encourage vertical and horizontal articulation of the facade on bases of buildings and incorporate detail at ground level through change of material, color, texture and architectural projections. Provide windows with clear glass throughout the building.



Policy 2: Provide in interior spaces such pedestrian amenities as plazas, places to sit, planting areas, fountains or cafes.

Policy 3: Incorporate architectural treatments in new buildings which would be sympathetic to the scale, form and proportions of older buildings, particularly those of outstanding quality.



Policy 4: Frame auto-oriented uses (such as gas stations) with a platform that relates harmoniously with nearby facade patterns and provide adequate ventilation and fire prevention design features.

Policy 5: Discourage bridges over minor streets or other public right-of-ways.

Pedestrian bridges often block views, shadow street and other open space areas and obstruct skyline elements while only marginally improving private pedestrian circulation between buildings. Providing safe, convenient and attractive pedestrian circulation at grade is a preferred alternative.

Policy 6: Design signs on new and renovated buildings to create a positive human scale along the street.

Signs should contribute to the creation of an attractive visual form and image for the street and should conform to the sign design guidelines described in Section VII of this Plan.

#### Open Space and Greenspace

OBJECTIVE: To develop a Greenspace System within the Sidewalk and Street Median Space which would Create a Distinctive Identity for the Avenue.

Policy 1: Incorporate both private and common open space and greenspace elements into new residential development and renovation of existing buildings to create a more attractive residential environment.

The open space/greenspace system should incorporate tree and other plantings in sidewalk and median strip spaces to give visual continuity to the street and to function as an organizing element integrating the numerous and diverse activity centers of the rather long and wide avenue. Additional plantings should be provided adjacent to minor streets, rear property lines and rooftops.

Policy 2: Assure that new development and major renovation contributes to the creation of an attractive street and sidewalk space by incorporating landscape vegetation, sidewalk pavement treatment, street lighting, and furniture in adjacent public spaces.

New development and building renovation projects should incorporate street space improvements which conform to the Landscape/Streetscape design guidelines established for the street (See Section VI).

#### Subarea Urban Design Objectives and Policies

##### Subarea 2: Civic Center (Hayes to Redwood Streets)

Policy 1: Strengthen the special space along Van Ness Avenue between Grove and McAllister Streets formed by the setback of City Hall and the Opera House/War Memorial buildings.

Ideally there should be a formal mid-block pedestrian crossing linking the entrance to City Hall with the park between the Opera House and the Veteran's Building. However, this may be infeasible because of traffic management

problems. In the absence of such a crosswalk, a small fountain, statue or other ceremonial feature at the midpoint in the median strip would be appropriate to visually link City Hall with the other two landmark buildings.

Policy 2: Strengthen the special ceremonial character of the Civic Center area.

The design of any new development at the southeast corner of Van Ness and Grove Street should contribute to the definition of this special space and the framing of the street and should incorporate design treatments which could complement adjacent outstanding structures. New development at the same height and scale as the Public Health Building and compatible in style with City Hall would fill a significant gap in the formal design of the Civic Center area.

Subarea 3: (Redwood to Broadway)

Policy 1: Assure that new development and major renovation at the Van Ness/California Street intersection are designed to minimize adverse wind conditions and maximize sun exposure at pedestrian level, particularly in the vicinity of the cable car terminus.

Policy 2: Preserve significant view corridors along east-west thoroughfares.

Along Pine, California, Sacramento, Clay and Washington Streets, new buildings should be stepped back above the podium level in order to preserve and strengthen the view corridor. In addition to the areawide urban design requirement for a 30 foot setback above the 40 to 60 foot podium level along Van Ness Avenue,

buildings should provide a 30 foot setback at the 40 to 60 foot podium level along the east-west thoroughfare frontage, except for buildings along California Street for which a 15 foot setback would be adequate to preserve significant views.

TRANSPORTATION AND CIRCULATION

Areawide Objectives and Policies

OBJECTIVE: To Provide Safe and Efficient Movement Among all Users on Van Ness Avenue.

The Van Ness Avenue right of way performs many functions. It is a primary state highway carrying heavy auto and truck traffic. Two Municipal Railway routes - the 47 and 42 lines - traverse most of its length as do Golden Gate transit buses. Appropriate measures are required to make certain this traffic flows smoothly. Measures must also be taken to assure that the sidewalk remain pleasant for the pedestrian and that the street will be as attractive a "front yard" for Van Ness Avenue residents as possible.

Transit Service

Policy 1: Reduce conflicts between transit vehicles and other moving and parked vehicles. Install bus bulbs at appropriate locations (see Figure 12) to reduce conflicts with vehicles and to improve passenger loading and unloading.

Policy 2: Improve synchronization of areawide traffic signals to facilitate a steady traffic flow.



Policy 3: Create transit only lanes during peak periods.

Policy 4: Provide clearly visible and readable street signs and bus stop signs to improve the legibility of bus stops for riders within the bus and for pedestrians. Such signage, however, should not overwhelm the design of the landscape/streetscape system. Provide safe and comfortable waiting areas for patrons by utilizing well-directed street lighting, bus bulbs and shelters.

Policy 5: Extension of the California Street Cable Car line to the Nihonmachi Center via California Street, Webster or Buchanan Street to Sutter Street has been proposed by MUNI and various citizen groups over the years in order to provide more efficient use of the Cable Car line as a transit system for residents as well as an attractive means of transporting visitors to special places of interest. Pending such an extension, provide an attractive terminus to the line at Van Ness Avenue.

#### Auto Circulation

Policy 1: Whenever feasible, provide access to parking from minor east-west streets.

#### Parking

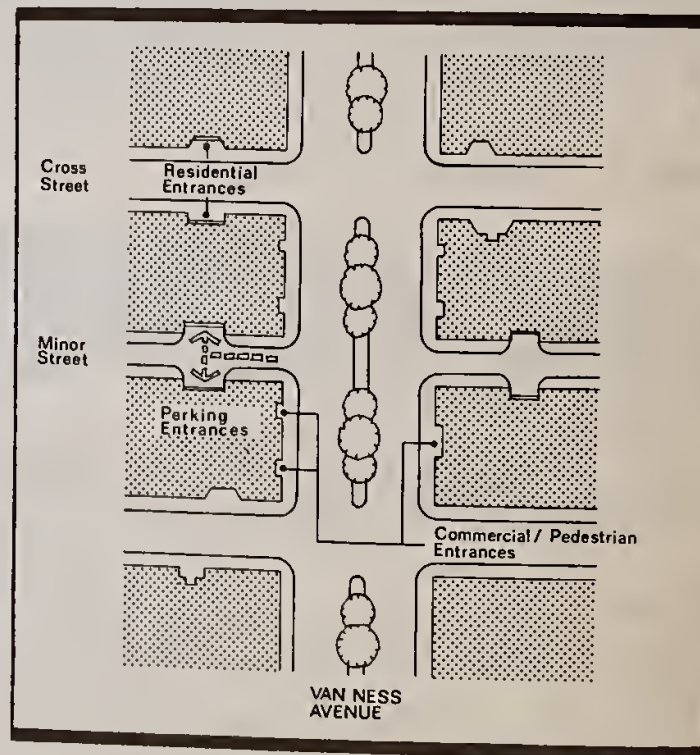
Policy 1: When justified by reduced demand, reduce parking requirements for new residential developments.

Policy 2: Adopt short-term parking rate structures for existing and new commercial parking resources to discourage commuter parking and provide visitor - shopper parking. Maximize the efficient use of existing and new accessory

parking resources by making these spaces available to the general public for use as short-term day or evening parking when not being utilized by the use to which it is accessory.

Policy 3: Encourage use of upper-story auto storage spaces within existing auto showrooms along Van Ness Avenue as community parking facilities for adjacent mixed use projects.

The use of these spaces for parking could be a highly desirable adaptive reuse of these buildings, a number of which are to be preserved under this Plan. Access to the parking should be from the side streets, not Van Ness.





## Pedestrian Circulation

Policy 1: Maximize the efficient and free flowing use of sidewalk space in new development.

Sidewalk space along Van Ness Avenue is shared by pedestrians, transit patrons, sidewalk elevators, street furniture, such as light fixtures, MUNI power poles, traffic signals, newsracks, benches and street trees, and moving vehicles entering or exiting an on-site parking area. Design of sidewalk space associated with new development should reduce clutter and pedestrian obstacles. Existing sidewalk elevators should be removed as quickly as possible and they should be prohibited in new developments. In order to maintain and improve the pedestrian pathway, curb cuts should be limited to those providing vehicular access to midblock parcels whose only access is from Van Ness.

Policy 2: Orient building entrances to enhance pedestrian circulation.

In addition to providing parking access for new developments fronting on Van Ness, the east-west minor streets should provide safe and attractive pathways for pedestrian travel. Major residential entrances should be designed very graciously and should front major east-west streets. Commercial entrances should be featured along Van Ness Avenue.

Policy 3: Unify the design of trash bins, benches, newsracks, street lighting fixtures, sidewalk surface treatment, canopies, awnings and bus shelters throughout the length of the street.

The design of street furniture should conform to the Van Ness Avenue Landscape/Streetscape and Sign design guidelines presented in Sections VI and VII of this Plan.

## Freight Loading

Policy 1: Discourage access to freight loading facilities from Van Ness Avenue.

Whenever possible, access to on-site freight loading spaces should be provided from minor or collector streets rather than from major thoroughfares. In instances where commercial uses front on Van Ness Avenue without convenient access to an off-street freight loading facility, priority should be given to allocating existing curb space on Van Ness Avenue to truck loading zones.

## RESIDENTIAL LIVABILITY

### Areawide Objectives and Policies

OBJECTIVE: To provide Safe and Attractive Environments within each Mixed Use Development.

### Safety, Security and Privacy

Policy 1: Maximize safety, security and privacy within new residential developments while encouraging efficient use of common open space areas.

Common areas associated with the residential portion of a mixed use development should be designed to prevent intrusion by commercial space users and the general public. Whenever possible, residential parking areas should be physically separated from public parking areas and should provide safe and secure passageway to residential elevators and walkways. Handicapped parking areas should be placed close to elevators and should be free of potential vehicle movement conflicts.

#### Sun, Shade and Wind Protection

Policy 2: Design housing projects to maximize sun orientation and natural light exposure to individual units. Incorporate design features which would provide wind protection and sun exposure to private and common open space areas.

#### Open Space/Greenspace

Policy 3: Generally maintain existing open space requirements for residential use.

The requirement of thirty-six square feet of open space for each dwelling unit should continue to apply in Subareas 3 and 4.

The setback above the podium level along the Van Ness Avenue frontage would substitute for the rear yard requirement for residential uses within Subarea 3. Development of the podium rooftop area as common useable open space could substitute for private open space at a ratio of 1.33 to 1 provided that this space were for the exclusive use of project residents. However, if an equal amount of additional open space is provided, the podium open space areas could be

shared by both residential and commercial tenants.

Policy 4: Allow common open space requirements to be met by a variety of recreation and open space features.

A common multipurpose room which may be used for weekday childcare and meeting, social and/or recreational areas for adults in the evenings and weekends, would be desirable in all large residential projects. Exercise rooms and low-cost recreational facilities should also qualify as required recreation open space.

#### Noise:

Policy 5: Design mixed use developments to create a quiet residential environment with a variety of intimate, personal spaces well insulated from the intrusion of exterior noises from street or commercial activities.

There are a variety of ways in which the sound coming from Van Ness Avenue can be mitigated to create an acceptable acoustic environment for dwellings. The required setback above the street wall will serve as a sound barrier for those units behind the setback. If necessary, noise control for interior spaces can be provided through sound-rated windows. Other sound attenuation measures such as deep balconies and solid balcony rails may also be necessary. Bedrooms or whole units can be insulated from noise by solarium which provide not only sound insulation but also usable garden space, and, on the south, solar heating. To encourage their use, solarium should be counted as required private, usable open space if they are designed so that a substantial portion of the window area can be opened to the air.



## V. OBJECTIVES AND POLICIES FOR CONSERVATION

### PRESERVATION OF ARCHITECTURALLY SIGNIFICANT BUILDINGS

#### Areawide Objectives and policies

OBJECTIVE 1: To Preserve the Fine Architectural Resources of Van Ness Avenue.

A number of existing buildings within the Van Ness corridor are of outstanding architectural quality and are important cultural and/or historic resources for the city. These buildings contribute positively to the character of the street and provide a basis for establishing a theme and identity for the area's new development. These buildings should be preserved, and new development should be designed to complement these older structures.

In the course of the economic evolution of the avenue, it is likely that many of these buildings will be converted to other uses. In particular, the function of the street as the auto show room for the city is likely to change in the near future, with many of the auto agencies proposing to relocate. Preservation and adaptive reuse of these rather specialized structures, a number of which are significantly smaller than the allowable zoning envelope of the site, will require flexibility and imagination.

Policy 1: Prevent demolition or inappropriate alteration of architecturally significant buildings.

Alteration of architecturally significant buildings should conform to guidelines for each building. Demolition should be limited to situations where it is clearly established that the building has been rendered unsafe and unoccupiable and infeasible for rehabilitation for occupancy due to fire, earthquake, flood or similar circumstances, or that substantial and irretrievable physical deterioration has occurred prior to adoption of the Van Ness Avenue Plan.

Policy 2: To the extent necessary to achieve preservation, allow relaxation in the residential use requirements and in the parking requirements.

Policy 3: When necessary to achieve preservation of the architecturally significant portions of certain buildings, allow additions to the structure.

In some cases, architecturally significant buildings can accommodate new development within the site's unused building envelope without adversely affecting the architectural value and structural integrity of the building.

In cases of penthouse additions above an existing building, the design of the project should incorporate adequate setbacks to insure that the building addition does not alter the proportions of the existing building facade. Such recessed additions can retain the integrity of the facade provided that the setback is adequate.



In cases of new buildings which are architecturally integrated with the significant portions of the original building, the new development should incorporate setbacks from the base of the original building, preserving significant facade treatments and an adequate number of bays and/or building depth to insure that the new development does not overwhelm the old and does not weaken or diminish the

significant interior or exterior features of the retained portion of the original building.

For buildings which are designated City Landmarks, alteration proposals would require compliance with the Certificate of Appropriateness procedure established in Article 10 of the City Planning Code.

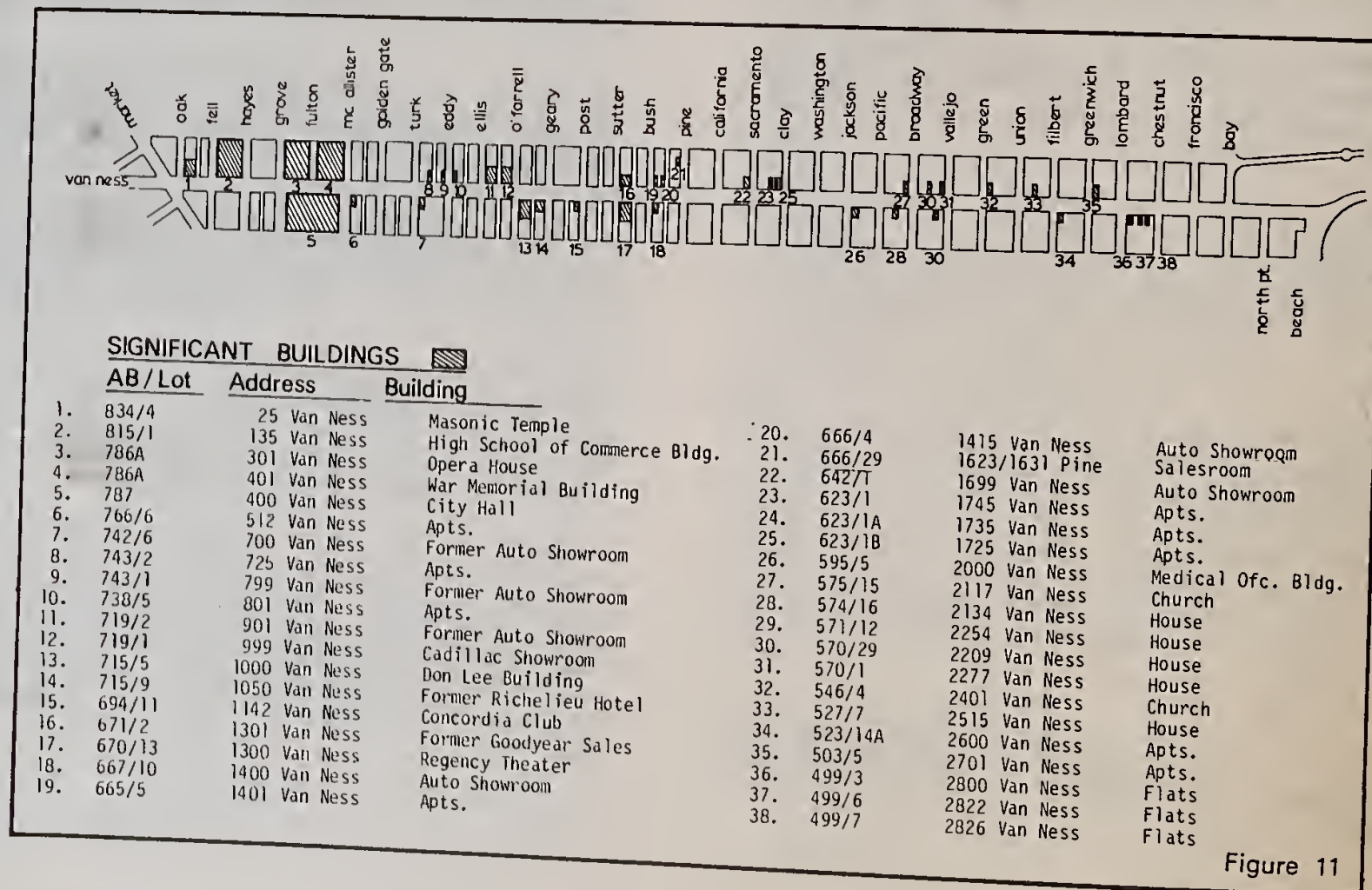


Figure 11

## PRESERVATION GUIDELINES FOR INDIVIDUAL BUILDINGS

Van Ness Avenue is endowed with a number of architecturally rich and attractive buildings which impart upon the avenue a special character and identity. Thirty-nine buildings have been identified as deserving special protection.

Although these buildings often share some common classical architectural features, each building is unique in its style and context, and guidelines for preservation of these buildings should be cognizant of each building's special resources and/or constraints. The special features of these buildings and guidelines for their potential adaptive reuse or alteration are briefly described below.

### 25 VAN NESS AVENUE, NORTHWEST CORNER OF OAK STREET (ASSESSOR'S BLOCK 834/LOT NO. 4)

#### MASONIC TEMPLE

A modified Florentine palazzo, this building has unusually fine detailing and imposing two-story arched windows. A most striking feature is the deep cornice, narrow corbelled arches and canopied statue projecting from the facade.

When necessary for preservation and adaptive reuse of the building, facade alterations should be permitted but limited to new, appropriately placed and scaled windows. Any vertical enlargement should be invisible from nearby street levels.

### 135 VAN NESS AVENUE, BETWEEN HAYES & FELL STREETS (AB 815/1)

Initially constructed as the High School of Commerce in 1924, this three story structure initiates the monumentally scaled Civic Center interval of Van Ness Avenue. Its Spanish decor is handsomely revealed in the window treatment of the third floor and in the reredos-like treatment of all three levels at the centered entry and in the tower on Hayes near Van Ness.

The three-story building fronting on Van Ness should be preserved along with the tower on the Hayes Street facade. Its block length horizontal scale is a great contribution to the Avenue and no exterior alterations visible from Van Ness should be permitted.

As a City Landmark, any alteration would require a Certificate of Appropriateness pursuant to Article 10 of the City Planning Code.





301, 400 and 401 VAN NESS AVENUE, BOTH SIDES  
BETWEEN GROVE AND McALLISTER STREETS (AB 786 and  
787)

#### City Hall, Opera House and Veteran's Building

French Renaissance in their inspiration, these three buildings constitute San Francisco's finest concentration of architectural significance. All three are designated City Landmarks.

No changes should occur to the facades or public rooms of any building unless established as necessary to the continued or expanded use of the facility. Should this prove necessary, new additions or alterations should be undertaken in the same idiom as that of the structure with matching materials and with the goal to achieve the same degree of success as accomplished in the Opera House addition on Franklin Street.



512 VAN NESS AVENUE, NORTHWEST CORNER OF  
McALLISTER STREET (AB 766/6)

#### Continental Court Apartments

This apartment building echoes the City Hall character in its color, stone finish, composition, its use of oversize French windows with transom (in the bays), its grill work masking the lower part of the windows and in its experience of entry which in City Hall is achieved through a colonnaded ante-room from which one emerges into the astonishing space of the rotunda. In 512 one proceeds through a skylit, exquisitely detailed colonnade into a surprisingly architecturally well-appointed lobby (also skylit) which appears to have retained its original detailing.

No alterations should be permitted above ground floor or to the entry or lobby. No vertical enlargements should be permitted. To be encouraged are alterations which would bring the ground floor into closer conformity to its original state and a uniform sign treatment appropriate to the building and its location.





700 VAN NESS, NORTHEAST CORNER AT TURK (AB 742/6)

Commercial Showroom

A pleasant two-story structure on a small site, its appearance could be much improved by consistent treatment in the ground floor arched openings on both streets as well as with a more sympathetic paint job.

The amount of solid wall surface is minimal in this building with only light-weight columns separating the arched ground floor openings, a condition which also prevails in the second story windows. The site size would not seem to permit a vertical addition with the kind of setbacks which would be necessary to maintain the architectural integrity and character of this building. Indeed, the almost tracery-like facades virtually beg that nothing of any weight be placed above them. If the building cannot be restored and retained as is, incorporation of the site into a larger parcel and development in accord with the design principles of this plan, recognizing its anchor position opposite the architecturally significant California Hall at the other end of this block on Polk, would be appropriate.



725 and 801 VAN NESS AVENUE, SOUTHWEST CORNER OF  
VAN NESS AT LARCH STREET (AB 743/2) AND  
NORTHWEST CORNER OF VAN NESS AT EDDY (AB 738/5)

Apartment Buildings

These are two buildings of five levels of apartments over ground floor commercial space. Both are liberally endowed with slanted bay windows, are architecturally pleasing to the eye, and provide mutual reinforcement when viewed together.

Both structures should be maintained with no exterior alterations to the residential floors and alterations at ground level, compatible with the upper floors, permitted only when necessary to satisfy the commercial needs housed inside.



799 VAN NESS AVENUE, SOUTHWEST CORNER AT EOOY  
STREET (AB 743/1)

Commercial Showroom

This two-story building is sandwiched (with intervening streets) between moderate-rise apartment buildings -- 725 and 801 Van Ness -- with which it has some commonalities despite the different scale. The chamfered corners at Eddy and Larch Streets reflect the angle of the slanted bay windows in the apartment buildings and the windows here and in 725 are multipaned. This building should be retained and used as a podium for a new vertical residential addition.

Unless the Larch Street level is used for parking, it might be necessary, if used commercially, to allow some modification to the windows on Van Ness as the existing windows on Van Ness do not permit visibility into the interior.

901 VAN NESS AVENUE, NORTHWEST CORNER OF  
O'FARRELL STREET (AB 719/2)

British Motors (originally Packard) Showroom

The architectural treatment of this building is decisively two-part. The monumental, columned, temple-like showroom facade on Van Ness returns along O'Farrell and Olive for two short bays and then the remainder of the facades on these two streets are industrial in appearance. In the Ellis Street facade, infill glazing between columns is an apparent recent alteration as its counterpart on Olive appears to be the original 1926 multipaned industrial sash. This building is a designated City Landmark and any alterations would require a Certificate of Appropriateness from the City Planning Commission.

Should the building be changed from its present use, the showroom facade on all three streets must be preserved and restored to its original colors, if not materials. No vertical enlargement should be allowed in this section. The ground floor showroom extends for a greater distance than the exterior facades would suggest. This room should be kept as near to



its original design and size as possible as it (and the interior of 1000 Van Ness) represents a most romantic period of California architecture -- the Spanish revival.

New construction to the rear of the showroom bays may be appropriate provided that it is sympathetic to the front part of the building. Penthouse additions should not be allowed above the architecturally treated portion of the building. Alterations and/or additions should be allowed west of the second bay if on the north side, the cornice height on the third bay is maintained and subsequent sections are designed to step up toward the west and the cornice lines and bay articulations are maintained on the south side. The highest development should be located on the northwest corner of the parcel. New development should be clearly separated from the original building by a recess or some other architectural treatment to maintain the integrity of the original building.

999 VAN NESS AVENUE, SOUTHWEST CORNER AT  
O'FARRELL STREET (AB 719/1)

Cadillac Showroom

Evidence suggests that this is the last of auto showrooms expressly built for that purpose on Van Ness. Its construction date was only 10 or 11 years after that of its neighbor, no. 901 across Olive Street, during which time buildings -- as with the merchandise sold therein -- became streamlined.

In this building, as with 901, the front bays should be preserved with new development permitted at the rear. Alterations or additions should not be allowed to the Van Ness facade or the first two bays to the west. Additions should be allowed west of the first two bays if they step up gradually to the west. The highest development should be located toward the southwest corner of the site. New development should be clearly separated from the original building by a recess or other architectural treatment to maintain the integrity of the original building and should maintain the existing bay width. All significant interiors features should be preserved intact.





1000 VAN NESS AVENUE, NORTHEAST CORNER AT  
O'FARRELL (AB 715/5)

One of the two great temples to the automobile on Van Ness Avenue and a designated City Landmark, the base, particularly the entrance, is monumental in scale and execution. The six upper floors, while bearing an excellent relationship to the base, take on architectural significance only when viewed with the base.

The interior fulfills the expectations gleaned from viewing the exterior. Monumentality is achieved and expressed in variations of the California architectural theme of the Spanish revival, also expressed by the cub bears atop columns on the Van Ness facade.

Alterations would require a Certificate of Appropriateness. No alterations should be allowed to the Van Ness facade and the first three bays returning on O'Farrell and Myrtle streets. Interior alterations should preserve the significant architectural features of the showroom; the auto service and storage areas to the rear of and above the showroom should be allowed minor alterations.

1050 VAN NESS AVENUE, SOUTHEAST CORNER AT GEARY  
STREET (AB 715/9)

Grosvenor Inn (formerly Richilieu Hotel)

Boasting an overscaled, intricately detailed cornice, this building is highly compatible in height, bulk and architectural treatment with the buildings to the east, which when taken together comprise about 75% of the block frontage.

No alterations to the exterior should occur above the ground floor. However, the sign band at the first floor level should be removed and replaced with a cornice or some other architectural feature which would be more sympathetic to the building. A more attractive color scheme for the building is also encouraged. Minor alterations to the ground floor are encouraged in order to bring it into closer conformity with its original state and as well as more sympathetic to the upper floors, and which would provide uses and window treatment which are more attractive to the pedestrian.



1142 VAN NESS AVENUE, SOUTHEAST CORNER AT POST  
STREET (AB 694/11)

Concordia Club

Currently undergoing reconstruction following severe fire damage, this classically inspired building bears a closer affinity to Civic Center buildings than to its automobile showroom and hotel neighbors. Nevertheless, it contributes positively to the avenue's monumental character and identity.

Over a rusticated ground floor facade, the upper two floors are treated as a single unit containing three two-story arched openings flanked by narrow pavillions at either end.

Exterior alterations should not be allowed except those which might bring the uppermost part of the facade back to its probable original state of being capped by a cornice or parapet.



1300 VAN NESS AVENUE, NORTHEAST CORNER AT SUTTER  
STREET (AB 670/13)

Regency Theater

Presently a theater and formerly a fraternal lodge, this building is designed in the manner of an Italian palazzo (which architectural treatment is also given the adjacent building to the east). No alterations to the architectural detailing of the facades should be allowed except as may be necessary at ground level for its continued viability as a commercial structure and to enhance its interaction with the pedestrian.



1301 VAN NESS AVENUE, NORTHWEST CORNER AT SUTTER  
STREET (AB 67172)

Commercial Showroom

A rare architectural style seldom seen in San Francisco (or elsewhere, for that matter), this Secessionist facade provides a high note of interest on Van Ness Avenue. The details have perhaps been exaggerated -- and at the same time diminished in importance -- by employing a multiplicity of colors on the building wall which emphasizes nearly every change in surface or geometric form. However, this is not a major detriment inasmuch as it can be easily remedied.

The architectural features along the Van Ness facade and approximately three bays along Sutter Street should be preserved. Development above the building may be appropriate provided that an adequate setback along the Van Ness (about 30 feet) and Sutter Street (about 15 feet) frontage is created. New development above and to the rear of the facade should be architecturally sympathetic with the building's significant features.



1400 VAN NESS, NORTHEAST CORNER AT BUSH STREET  
(AB 667710)

This is a fine restrained Classical Revival auto showroom with a rusticated base and Corinthian pilasters. The bay and pilaster width relate to the width of the street, with the Van Ness facade having a larger scale than the Bush facade.

Because of the building's corner location it would be difficult to alter or add to without significantly harming its integrity, therefore the building should remain intact.





1623 & 1631 PINE STREET, SOUTH SIDE OF FRANKLIN,  
WEST OF VAN NESS (AB 666/29)

Commercial Buildings

These intriguing facades, not quite twins, were constructed in 1905 (no. 1623) and ca. 1908 (no. 1629) to attract the carriage trade of fashionable Van Ness Avenue and Pacific Heights. Originally the buildings housed Kelly's Stables which offered "fine equipages, livery and boarding stables". By 1911, the buildings housed automobiles. Gradually, however, the buildings assumed other automotive-oriented uses until today, after interior reconstruction and alterations, they are used for offices and the sale of various merchandise.

Both are three stories over basement and intended to be temple-like in their eclecticism in which they preceded their automobile counterparts by a couple of decades.

Architectural embellishments and their arrangement on both are similar and in many instances identical. The lower two stories of each are divided, into three parts in no. 1623 and five in no. 1629, by two-story corinthian pilasters. Each center section contains a tympanum above the second floor and an arched entry at ground level. The uppermost portion of the facade of 1623 appears to have had some of its ornamentation removed.

The facades of both buildings should be maintained and appropriate restoration of 1623 is encouraged.

1401 VAN NESS, NORTHWEST CORNER AT BUSH STREET  
(AB 666/5)

This is a seven story post-fire apartment building. Its facade is richly articulated by bay windows, intact sixth story and roof cornices, and an ornamental fire escape.

Because of its corner location and two architecturally treated facades, it would be difficult to alter or add to without significantly harming its integrity, therefore it should be preserved intact.



141S VAN NESS, NORTHWEST CORNER AT AUSTIN STREET  
(AB 666/4)

A post-fire auto showroom built around 1909, the building retains its original use. Designed in an Italianate commercial style, the building retains its original bracketed cornice, floral-motif frieze, and the Ionic pilasters.

The architectural treatment extends two bays up Austin Street, allowing flexibility for alterations or additions to the rear of the building, provided that the new structures step up to the west and maintain the existing bay width and cornice line.

1699 VAN NESS, SOUTHWEST CORNER AT SACRAMENTO  
STREET (AB 642/1)

This building is a four story auto showroom originally built for the Paige Motor Car Company and is still used as an auto showroom. The building is marked by two story arches on the showroom level differentiating between the functions of the building. The building has been placed on the National Register of Historic Places.

Because of its corner location, it could not be altered or added to without significantly harming its integrity, and therefore it should remain intact.

172S, 173S, and 174S VAN NESS, WESTSIDE, BETWEEN  
SACRAMENTO AND CLAY STREETS (AB 623/1B, 1A and 1)

An excellent collection of three Gothic apartment buildings strongly related to each other by the use of similar materials, color, massing, scale, fenestration, bays, cornice and belt lines. 174S Van Ness is located at the corner of Van Ness and Clay and the Clay facade serves to tie this group to a number of similar apartment buildings on Clay.

Because alterations and additions would harm its integrity, the corner building (174S Van Ness) should be preserved intact. 172S and 173S Van Ness could receive minor alterations or additions to the rear without harming their integrity.



2000 VAN NESS AVENUE, NORTHEAST CORNER AT  
JACKSON STREET (AB 595/5)

Medical Arts Building

A seven story medical office building with ground floor retail use. The base consists of the lower two floors in which paired windows are separated vertically by two-story Corinthian pilasters and separated horizontally by spandrels embellished with bas-relief ornamentation. Above the third floor windows a prominent belt runs the lengths of both facades and a cornice caps the top edge.

Because of its corner location, its present massing and its two highly visible, architecturally outstanding facades, no exterior alterations should be permitted.



2117 VAN NESS, SOUTHWEST CORNER AT BROADWAY (AB  
575/15)

This is an important corner stone church designed in the Romanesque style. It is noteworthy for its entrance arch decoration, corner tower which is an important visual element in the streetscape, and an excellent second story arcade on the eastern facade.

Because of its massing and architectural treatment on four facades, it would be difficult to alter or add to without significantly harming its integrity, and therefore it should be retained intact.





2134 VAN NESS, EASTSIDE, BETWEEN PACIFIC STREET  
AND BROADWAY (AB 574/16)

A two story Craftsman style residence converted to offices, with a clinker brick base and half-timber influences in the upper story. The facade is capped by two steep gables.

Because of its small scale, and facade treatment along its north side, it would be difficult to alter or add to without significantly harming its integrity and therefore should be retained intact.



2209 VAN NESS, WESTSIDE, NORTH OF BROADWAY (AB  
570/21)

Located on a small rise and set back from the street, this is a three and a half story Palladian residence with a strongly emphasized central bay consisting of giant order ionic columns flanking the entrance, a second floor balcony, Palladian window on the second story and the roof dormer, and an ornate cornice.

Because it is a freestanding structure, it would be difficult to alter or add to without significantly harming its integrity and therefore it should remain intact.



2254 VAN NESS, EASTSIDE, SOUTH OF VALLEJO STREET  
(AB 571/12)

Located on a small rise and visible above a one story building to the north, this is a three story high Victorian residence with stick style influences, a steeply-pitched roof, an ornate chimney stack and ornate detailing.

Because of its prominent location, its three architecturally treated facades, and its massing, it would be difficult to alter or add to without significantly harming its integrity and should remain intact.



2277 VAN NESS, SOUTHWEST CORNER AT VALLEJO  
STREET (AB 570/1)

This is a three and a half story Victorian residence recently converted to commercial use with a two story addition to the rear. The building has a strong presence on the street due to its prominent corner location, its square bays windows on both Van Ness and Vallejo, and a complicated gable roof.

Because of its corner location and two architecturally treated facades, only alterations or additions to the rear addition are possible without significant harm to the original structure's integrity. Additions to the rear should be kept below the level of the original cornice.



2401 VAN NESS, NORTHWEST CORNER AT GREEN STREET  
(AB 456/4)

This is an interesting small scale Byzantine style church with Baroque detailing. The style, massing and central form of the church are unusual in the Bay Area.

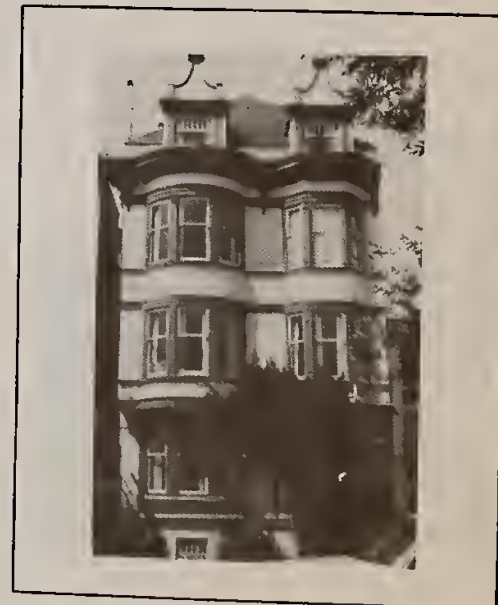
Because of its corner location, three architecturally treated facades, and massing, it would be difficult to alter or add to without significantly harming its integrity and therefore it should remain intact.



2517 VAN NESS, WESTSIDE, NORTH OF UNION STREET  
(AB 527/7)

This is a three and a half story Victorian residence with an extremely articulated handling of the facade consisting of round and three sided bays, an impressive entrance flanked by double Ionic columns, ornate cornices and belt course, and an unusual treatment of the roof dormers.

Because of massing and scale, the building could not be altered without significant harm to its integrity and it should remain intact.





2600 VAN NESS AVENUE, NORTHEAST AT FILBERT (AB  
523/14A)

Apartment Building

This is a well-proportioned six story apartment building with good detailing throughout its facades. Above the rusticated basement, a belt course follows the plan outline of the facades including the tiers of bay windows which are separated horizontally by spandrels with bas-relief. A handsome cornice completes the architectural excellence of the building.

Because of its corner location and two architecturally significant facades, no exterior alterations should be permitted.



2701 VAN NESS, NORTHWEST AT GREENWICH STREET (AB  
503/5)

Apartment Building

This is a fine Chateausque seven story apartment building located at the northwest corner of Van Ness and Greenwich. It is richly decorated with a two story entrance flanked by pilasters, mansard roof, octagonal chimineys and bays.

With its fine design and prominent corner location at the foot of a hill, the building could only accept minimal alterations without harming its integrity; a small penthouse or minor additions to the northwest corner may be appropriate to tie it in with a new development to the north.



2800 VAN NESS AVENUE, NORTHEAST CORNER AT  
LOMBARD STREET/(AB 499/3)

This three story Victorian residence forms part of a fine group of pre-fire buildings between Lombard and Chestnut which retains the scale and character of pre-fire Van Ness Avenue. Although two of the five buildings have been altered, the group remains coherent through use of similar scale and massing.

Because of its corner location and two architecturally treated facades, it could not be altered or added to without significant harm, and therefore it should be retained intact.



2822 VAN NESS AVENUE, EASTSIDE, NORTH OF LOMBARD  
STREET/(AB 499/6)

This is a three and a half story Victorian residence with a simple facade framed by two story pilasters, which is part of the group of pre-fire buildings on this section of Van Ness.

Because it is a freestanding structure, it could not be altered or added to without significant harm, and therefore it should be retained intact.



2826 VAN NESS AVENUE, EASTSIDE, SOUTH OF  
CHESTNUT STREET/(AB 499/7)

This is a three story Victorian residence, which is part of the group of pre-fire buildings on this section of Van Ness.

Because it is a freestanding structure, it could not be altered or added to without significant harm, and therefore it should be retained intact.



## PRESERVATION OF HOUSING RESOURCES

### Areawide Objectives and Policies

OBJECTIVE: To Conserve Existing Moderate-to High-Density Housing Resources.

Policy 1: Preserve existing moderate to-high-density housing structures unless demolition will result in a substantial increase in on-site housing.

Existing housing is an important source of affordable housing and rental housing, both of which are important resources to retain. Existing moderate or high-density housing, which has an efficient site utilization ratio (encompassing more than 60% of the site's allowable building area), should be preserved in residential use.

Based on that ratio, the following apartment buildings (some of which are architecturally significant) should be retained as housing (although the ground floor should be allowed to convert to retail use).

512 Van Ness .....	40 units
725 Van Ness .....	30 units
801 Van Ness .....	37 units
840 Van Ness .....	50 units
1356 Van Ness .....	27 units
1401 Van Ness .....	84 units
1617 Pine .....	40 units



1725 Van Ness .....	24 units
1735 Van Ness .....	36 units
1745 Van Ness .....	49 units
2128 Van Ness .....	28 units
2146 Van Ness .....	14 units
2160 Van Ness .....	14 units
1590 Broadway .....	268 units

In addition, existing housing in Subarea 4 (Broadway to Bay Street) meets that ratio and should be retained with carefully designed, moderate-density "in-fill" housing encouraged on parking lots or other sites occupied by wholly commercial structures.

The loss of existing housing structures which are not architecturally significant and are on sites not meeting the site utilization ratio should not be resisted where demolition would result in construction of a substantial net increase (a minimum of 200%) in the number of housing units on-site and low and moderate income units are replaced on-site.

## VI. LANDSCAPE/STREETSCAPE DESIGN GUIDELINES

### OBJECTIVES

OBJECTIVE 1: To Create an Attractive Street and Sidewalk Space which would Contribute to the Transformation of Van Ness Avenue into a grand Boulevard.

### POLICIES

#### Plantings

Policy 1: Require sponsors of major renovation or new development projects to improve and maintain the sidewalk space abutting their properties according to the guidelines contained in this Plan.

Policy 2: Where there are no trees, plant trees within the sidewalk space and the median strip. Maintain existing healthy trees and replace unhealthy ones.

Trees should be planted on center two feet from the curb, an average of 20 foot spacing along Van Ness Avenue. At major intersections, the vertical trees might be grouped. Trees should be planted in a minimum 20 gallon container with horizontal root deflectors within the sidewalk or median space and should be at least four-inch caliper, 10 feet tall, have good taper, and be free of disease and pests.

Appropriate vertical trees include the Eucalyptus Sideroxylon (Red Iron bark), Eucalyptus Polyanthemus (Silver Dollar Euc.) and Liquidamber Stryacifula (Sweet Gum) species.

Appropriate canopy trees include the Eucalyptus Ficifolia, Pittosporum Undulatum (Victorian Box) and Acacia Longifolia (Golden Wattle) species.

Policy 3: Incorporate low-growing ground cover around the tree plantings within the median strip.

Shrubs or hedges growing to a maximum height of three feet may be planted in addition to or in lieu of ground cover within the median strip. No shrubs or hedges should be planted within sidewalk space. Ground cover species should be consistent with other plantings in the median along Van Ness.

Policy 4: Provide street trees with tree grates with removable sections to adequately accommodate tree growth.

The design of the tree grates should be uniform throughout the length of the street.

### Sidewalk Treatment

Policy 1: Maintain existing sidewalk widths.

Policy 2: Incorporate uniform sidewalk paving material, color, pattern and texture throughout the length of the Avenue.

Sidewalk and median strip paving materials should be concrete, light grey-tone in color, with a plain, brushed surface texture, except for a darker grey 12-inch curbside trim which should add a richness in color and texture to the avenue.

Policy 3: Trim sidewalk and median strip curbs with hydraulically pressed, pre-cut four-inch square stone paving blocks to a horizontal depth of 12 inches.

The stone pavers should be of a complementary medium grey-tone color (e.g., Hanover Prest Paving R.D. No. 4).

Policy 4: Replace existing bus stops with bus bulbs (widened sidewalk space) at the following 30 locations along Van Ness Avenue:

#### WESTSIDE

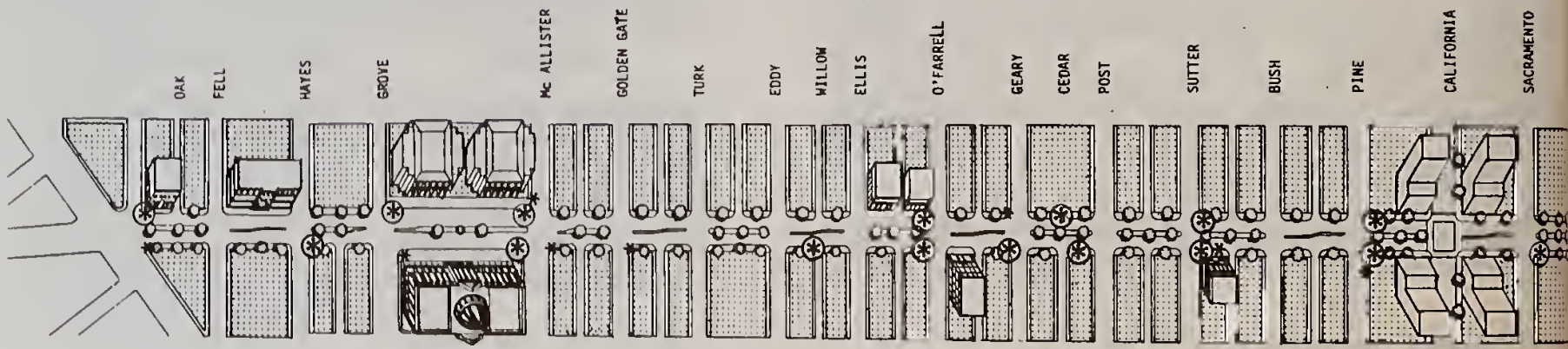
No. of Bay Street  
No. of Chestnut Street  
No. of Greenwich Street  
No. of Union Street  
No. of Vallejo Street  
No. of Pacific Street  
No. of Washington Street

No. of Pine Street  
No. of Sutter Street  
Midblock between post  
Street and Geary Blvd.  
South of O'Farrell Street  
South of Willow Street  
South of McAllister Street  
No. of Grove Street  
No. of Oak Street

#### EASTSIDE

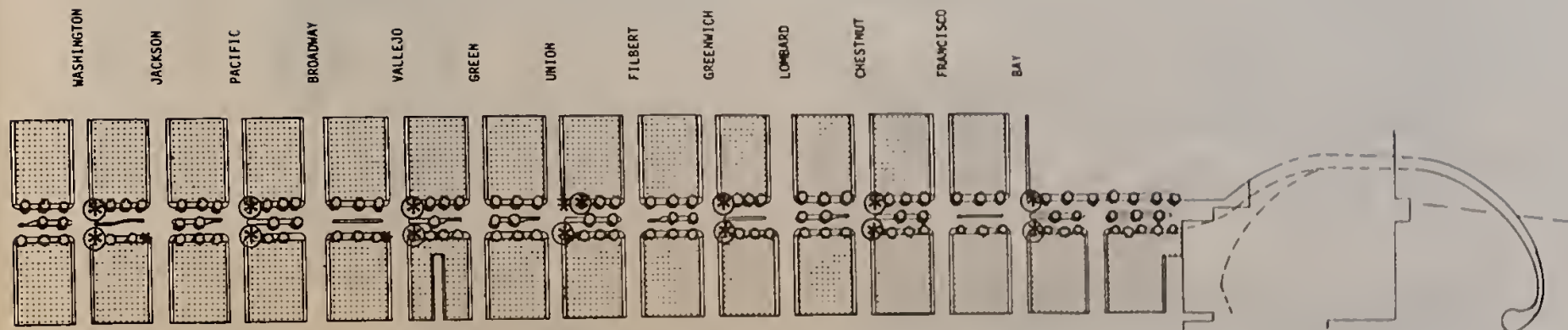
No. of Bay Street  
No. of Chestnut Street  
No. of Greenwich St  
No. of Union Street  
No. of Vallejo Street  
No. of Pacific Street  
No. of Washington St.  
No. of Sacramento St.

No. of Pine Street  
No. of Sutter Street  
No. of Cedar Street  
South of O'Farrell St.  
South of Willow Street  
South of McAllister St.  
No. of Hayes Street





# LANDSCAPE PLAN



## LEGEND

- TREES
- ⌋ GROUND COVER IN MEDIAN
- ⊛ BUS BULB LOCATION
- \* NEWSRACKS LOCATION

FIGURE 12

## Street Lighting

Policy 1: Assure a uniform architectural style, character and color in the design of street light poles. Existing street light poles should be maintained and enhanced in order to contribute to the special identity of the avenue.

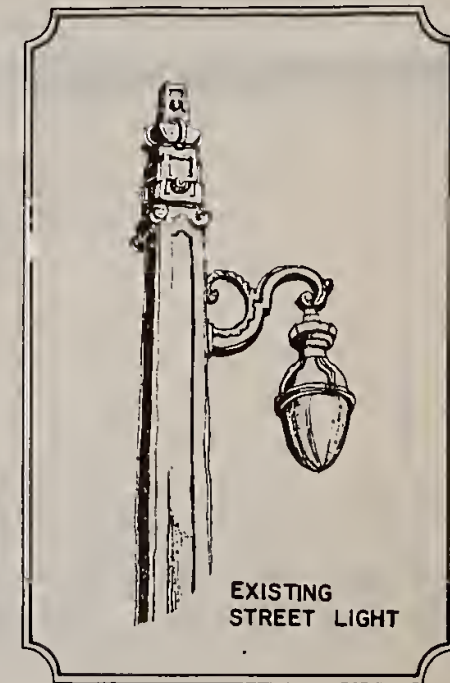
Painting all the light poles along Van Ness Avenue a blue and gold color scheme, similar to that of the Civic Center light poles, would contribute to this special identity.

Policy 2: Convert existing lamps and electrical circuitry to more economical, energy-conserving types of lamps and voltage.

New street lamps should emit a soft, white light and should complement the new energy efficient system.

Policy 3: Design the angle and color of illumination on existing and new street lights to minimize glare to nearby residential uses.

Lighting should not damage adjacent landscape plantings and should provide safe and attractive lighting for pedestrians.



## Street Furniture:

Policy 1: Design street furniture to be attractive, safe, compact, durable, easy to install and maintain, and place them at convenient locations and intervals throughout the length of the street.

Policy 2: Maintain existing bus shelters.

New shelters or replacement shelters should be placed between the trees along the treeline of the sidewalk.

Policy 3: Attach benches to the ground, and locate them between the trees along the treeline of the sidewalk adjacent to bus stops.

Policy 4: Cluster Newspaper racks at specific corner locations.

News racks should be fixed to the ground and not attached to utility poles, or other street furniture; should not, as a clustered group, exceed six feet in length; and should provide an adequate visual clear zone at intersections for motorists' turning movements and adequate space within the sidewalk for free flow pedestrian circulation.

Newspaper racks should be clustered only at the intersections in the sidewalk space at the following locations along Van Ness Avenue and would not be allowed elsewhere.

#### WESTSIDE

South of McAllister  
South of Geary  
North of Pine  
South of Jackson  
North of Union

#### EASTSIDE

North of Market  
North of Grove  
North of McAllister  
North of Golden Gate  
South of Geary  
North of Sutter  
South of Vallejo

### VII. SIGN DESIGN GUIDELINES

OBJECTIVE: To provide identification of commercial and/or residential uses within specific developments while contributing positively to the overall visual identity of the street.

### POLICIES

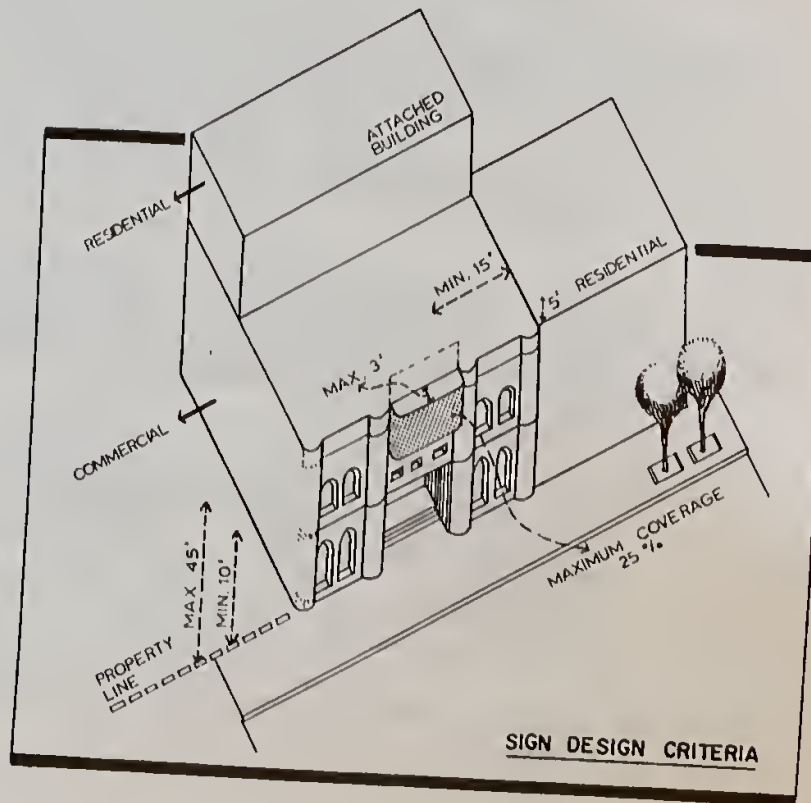
Policy 1: Permit business signs and other identifying signs as defined in Article 6 of the City Planning Code as a principal use within the Van Ness Avenue Plan area. Prohibit general advertising signs and/or flashing or blinking signs.

Policy 2: Require permitted signs to fulfill the following design criteria.

- a) Signs should not be less than 10 feet above grade, should not be higher than 45 feet above the sidewalk and should not project more than three feet from the property line or building face.
- b) Signs should not project above the height of the building to which it is attached.
- c) Signs should not project within five feet of residential uses in the vertical dimension and should not project within 15 feet from any residential use in the horizontal dimension.
- d) Signs should have as much transparency as possible in order to emphasize the buildings architecture. A goal of 80% transparency for each sign is recommended. The information density of each sign should be minimized.
- e) Signs should not be placed in front of windows.



- f) Counting all signs on an individual building, coverage should not exceed 25% of the base of the building up to a height of 50 feet but should not be less than a minimum 16 gross square feet per storefront.
- g) Political signs should be allowed only as a temporary use pursuant to Article 6, Section 603(c) of the City Planning Code.



VAN NESS AVENUE PLAN  
Acknowledgements

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CONSULTANTS - Special Thanks to:

Boris Dramov, Bonnie Fisher and Jim Adams of ROMA for their assistance in formulating the Plan's Urban Design Concepts.

Michael Corbett and Grant DeHart of the Foundation for San Francisco's Architectural Heritage, for their assistance in identifying and rating significant buildings.

William Kostura, for his assistance in land use, infrastructure, public services and architectural and historical resources research.

Mark Schatz of Bull, Volkmann, Stockwell and Jeffrey Heller and Javier Miquel of Kaplan, McLaughlin, Diaz for their assistance in Urban Design concepts.



*[Faint, illegible handwritten text, possibly bleed-through from the reverse side.]*





